



Meriden-Markham Municipal Airport

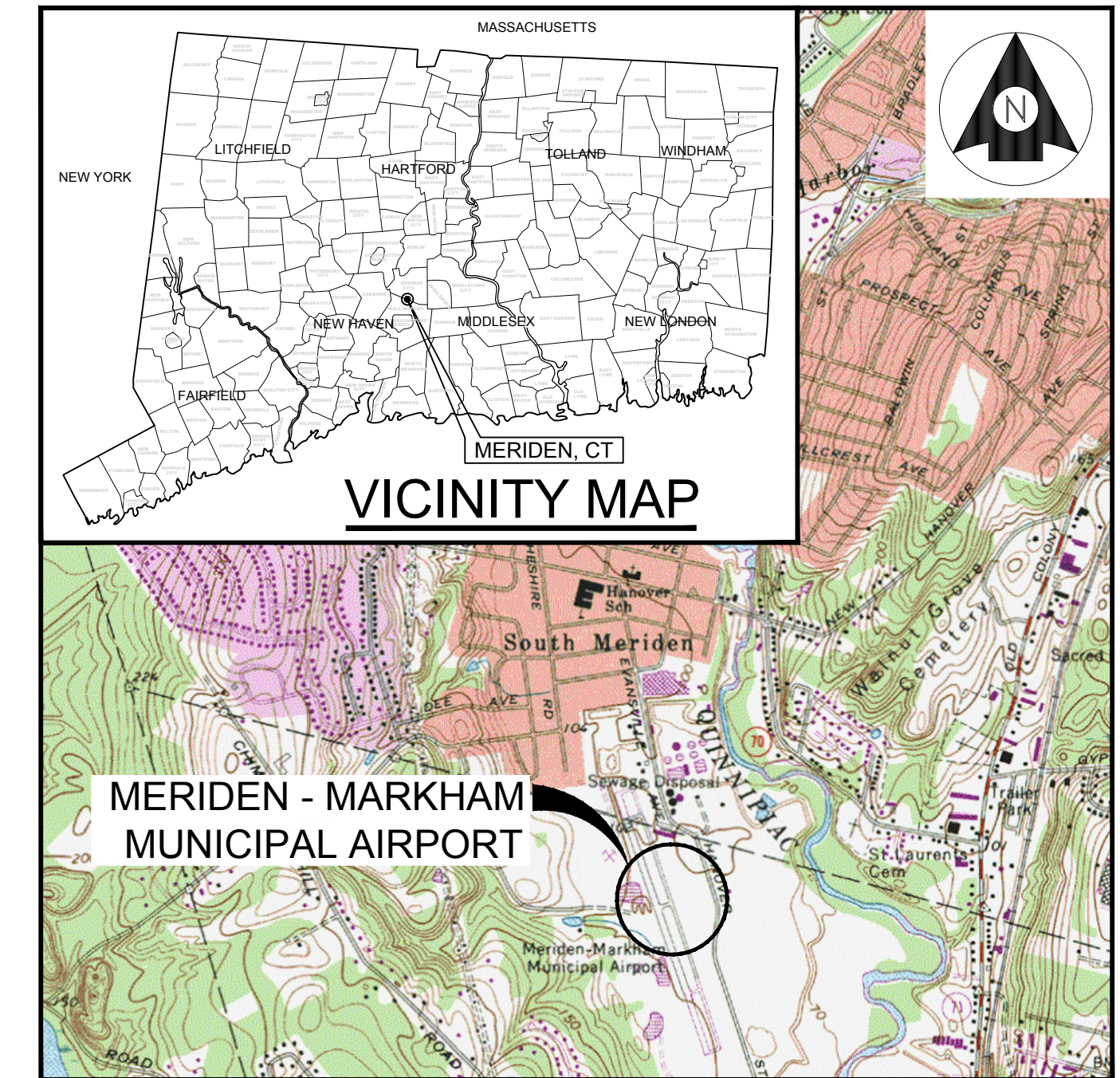
City of Meriden, Connecticut

Contract Plans For:

MID-BASE APRON REHABILITATION



City of Meriden Contract No. B019-XX
 FAA AIP NO. 3-09-0012-022-2019
 Stantec Project No. 195211190
 MAY 9, 2019



PROJECT LOCATION MAP
 NOT TO SCALE

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GENERAL NOTES

- THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CONTRACT PLANS AND SPECIFICATIONS REFERENCED THEREIN.
- THE PROJECT SHALL BE CONSTRUCTED IN A TIMELY MANNER IN ACCORDANCE WITH THE CONTRACTOR'S APPROVED PROJECT SCHEDULE. THE SCHEDULE SHALL PROVIDE FOR COMPLETION OF THE WORK AS SHOWN ON THE PLANS AND DESCRIBED IN THE CONTRACT SPECIFICATIONS.
- MERIDEN-MARKHAM MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF ALL WORK WITH THE AIRPORT MANAGER AND THE ENGINEER IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- CONTRACTOR'S STAGING AREA - AN AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE. THE CONTRACTOR'S STAGING AREA SHALL BE RESTORED TO ITS PRE-CONSTRUCTION CONDITION UPON COMPLETION OF USE, AT THE CONTRACTOR'S EXPENSE.
- EXISTING AIRFIELD LIGHTING SYSTEMS - INTERRUPTION OF EXISTING AIRFIELD LIGHTING SYSTEMS NOT INCLUDED IN THIS PROJECT SHALL NOT BE PERMITTED. ALL AIRFIELD LIGHTING CIRCUITS AFFECTED BY THIS PROJECT SHALL BE MAINTAINED BY THE CONTRACTOR DURING OPERATIONAL PERIODS IN ACCORDANCE WITH THE SPECIFICATIONS AND/OR AS DIRECTED BY THE ENGINEER.
- ALL MATERIALS AND EQUIPMENT WHEN NOT IN USE SHALL BE PLACED IN APPROVED AREAS WHERE THEY WILL NOT CONSTITUTE A HAZARD TO AIRCRAFT OPERATIONS AND NOT PENETRATE CLEARANCE SURFACES. EQUIPMENT SHALL BE PARKED AT THE STAGING AREA WHEN NOT IN USE.
- FOR EMERGENCIES INVOLVING SAFETY (INJURIES, FIRES, SECURITY BREACHES, ETC.) THE CONTRACTOR SHALL MAKE DIRECT CONTACT WITH THE AIRPORT MANAGER (203) 630-2878 FOLLOWED BY NOTIFICATION TO THE ENGINEER AS SOON AS POSSIBLE.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- IN ACCORDANCE WITH THE SPECIFICATIONS, LABOR WAGE RATES SHALL BE POSTED ON SITE IN A WEATHERPROOF ENCLOSURE.

CONSTRUCTION SAFETY NOTES

- ALL CONSTRUCTION FOR THIS PROJECT SHALL CONFORM TO THE GUIDELINES SET FORTH IN FEDERAL AVIATION ADMINISTRATION (FAA) AC150/5370-2F "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", AND THESE PLANS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL COMPLY WITH ALL SECURITY REQUIREMENTS SPECIFIED HEREIN. THE CONTRACTOR SHALL DESIGNATE IN WRITING TO THE OWNER THE NAME OF ITS "CONTRACTOR SECURITY OFFICER". THE CONTRACTOR SECURITY OFFICER SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS OF THE CONTRACT. THE CONTRACTOR'S SECURITY OFFICER SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR PERSONNEL ON THESE REQUIREMENTS.
- THE CONTRACTOR SHALL ACQUAINT ITS SUPERVISORS AND EMPLOYEES WITH THE AIRPORT ACTIVITIES AND OPERATIONS THAT ARE INHERENT AT THIS AIRPORT AND SHALL CONDUCT ITS CONSTRUCTION ACTIVITIES TO CONFORM TO ALL ROUTINE AND EMERGENCY AIR TRAFFIC REQUIREMENTS AND TO THE GUIDELINES ON SAFETY.
- CONSTRUCTION ACTIVITIES ARE NOT PERMITTED WITHIN THE SAFETY AREA/OBJECT FREE AREA OF AN ACTIVE RUNWAY, TAXIWAY OR APRON TAXILANE WITHOUT THE APPROVAL OF THE AIRPORT MANAGER (SEE AC NO. 150/5370-2F, SECTION 3-3).
- IN ORDER FOR THE CONTRACTOR TO OPERATE WITHIN THE AIR OPERATIONS AREA, APPROPRIATE NOTICES TO AIRMEN (NOTAMS) MUST BE ISSUED BY THE AIRPORT MANAGER THROUGH THE FAA FLIGHT SERVICE STATION. THESE NOTICES PROVIDE INFORMATION ON CLOSED, LIMITED, OR HAZARDOUS CONDITIONS TO AIRMEN AND USERS OF THE AIRPORT. A 48-HOUR NOTICE IS REQUIRED FOR ISSUANCE OF THE NOTAM. ALL CONSTRUCTION OPERATIONS MUST BE CLOSELY COORDINATED WITH THE AIRPORT MANAGER FOR NOTAM ISSUANCE.
- ALL CONTRACTOR VEHICLES, INCLUDING HAULING VEHICLES, THAT ARE AUTHORIZED TO OPERATE WITHIN THE SECURITY FENCE ON THE AIRPORT WITHIN THE DESIGNATED LIMITS OF CONSTRUCTION AS DEFINED HEREIN, SHALL DISPLAY IN FULL VIEW ABOVE THE VEHICLE A 3' x 3' OR LARGER ORANGE AND WHITE CHECKERBOARD FLAG, EACH CHECKERBOARD COLOR BEING 1" SQUARE. WHEN OPERATING DURING PERIODS OF DARKNESS OR LIMITED VISIBILITY, CONTRACTOR'S VEHICLES SHALL BE EQUIPPED WITH ROTATING OR FLASHING AMBER LIGHTS.
- OPEN-FLAME WELDING OR TORCH-CUTTING OPERATIONS ARE PROHIBITED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED IN ADVANCE BY THE AIRPORT MANAGER.
- OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING RED LIGHT UNITS (ACCEPTABLE TO THE AIRPORT MANAGER) DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS.
- STOCKPILED MATERIAL SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT AS A RESULT OF AIRCRAFT OR WIND CONDITIONS IN EXCESS OF 10 KNOTS.
- CONTRACTOR GENERATED DEBRIS, WASTE, AND LOOSE MATERIAL (INCLUDING DUST AND DIRT) CAPABLE OF CAUSING DAMAGE TO AIRCRAFT LANDING GEAR OR PROPELLERS, OR BEING INGESTED IN JET ENGINES, SHALL NOT BE ALLOWED ON ACTIVE AIRCRAFT MOVEMENT AREAS OR ADJACENT GRASSED AREAS. MATERIALS OBSERVED TO BE WITHIN THESE AREAS SHALL BE REMOVED IMMEDIATELY BY THE CONTRACTOR.
- WHERE TRAVEL ON OR ACROSS RUNWAYS, RAMP AREAS, TAXIWAYS, OR AIRCRAFT APRONS IS REQUIRED, THE CONTRACTOR SHALL PROVIDE ADEQUATE PERSONNEL AND EQUIPMENT TO KEEP SUCH SURFACES CLEAR OF DEBRIS.
- THE CONTRACTOR SHALL PROVIDE FLAGMAN AS REQUIRED FOR ALL OPERATIONS INCLUDING EQUIPMENT ENTERING & LEAVING ADJACENT ROADWAYS. THERE IS NO DIRECT PAY FOR THE FLAGMEN.

- THE CONTRACTOR SHALL PROVIDE, INSTALL AND MAINTAIN FLASHING LIGHTS AND BARRICADES WHEREVER OPEN EXCAVATIONS OR IRREGULAR GRADES ARE LEFT WITHIN THE SAFETY AREA OF AN ACTIVE TAXIWAY OR WHERE TEMPORARY PAVEMENT CLOSURES OR AIRCRAFT LIMITATIONS ARE REQUIRED. BARRICADES SHALL BE SPACED NO MORE THAN 8 FEET ON CENTER OR AS NOTED ALONG THE AFFECTED PAVEMENT EDGE. THE CONTRACTOR SHALL DAILY MAINTAIN THE LIGHTS AND BARRICADES IN AN OPERABLE CONDITION FOR THE DURATION OF THE PROJECT. THE CONTRACTOR SHALL FURNISH THE OWNER A CONTACT NUMBER FOR 24-HOUR MAINTENANCE OF LIGHTS AND BARRICADES.
- THE CONTRACTOR SHALL PERFORM CONSTRUCTION OPERATIONS AS NECESSARY TO MINIMIZE ATTRACTION TO BIRDS CAUSED BY PONDED WATER AND GRASS SEED.
- PROTECTION OF ALL PERSONS SHALL BE PROVIDED THROUGHOUT THE PROGRESS OF THE WORK. THE WORK SHALL PROCEED IN SUCH A MANNER AS TO PROVIDE SAFE CONDITIONS FOR ALL WORKERS INCLUDING AIRPORT PERSONNEL AND THE ENGINEER'S REPRESENTATIVES. THE SEQUENCE OF OPERATION SHALL BE SUCH THAT MAXIMUM PROTECTION IS AFFORDED TO INSURE THAT PERSONNEL AND WORKERS IN THE WORK AREA ARE NOT SUBJECT TO ANY DANGEROUS CONDITIONS.
- DURING PERFORMANCE OF THIS CONTRACT, THE AIRPORT RUNWAY, TAXIWAYS, FUELING AREAS, AND AIRCRAFT PARKING APRONS SHALL REMAIN IN USE BY AIRCRAFT TO THE MAXIMUM EXTENT POSSIBLE. ALL AIRCRAFT TRAFFIC ON THESE AREAS SHALL HAVE PRIORITY OVER CONTRACTOR'S TRAFFIC. THE AIRPORT MANAGER RESERVES THE RIGHT TO ORDER THE CONTRACTOR AT ANY TIME TO VACATE ANY AREA NECESSARY TO MAINTAIN SAFE AIRCRAFT OPERATIONS. USE OF AREAS NEAR THE CONTRACTOR'S WORK WILL BE CONTROLLED TO MINIMIZE DISTURBANCE TO THE CONTRACTOR'S OPERATION. THE CONTRACTOR SHALL NOT ALLOW EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, OR ANY OTHER UNAUTHORIZED PERSON TO ENTER OR REMAIN IN ANY AIRPORT AREA WHICH WOULD BE HAZARDOUS TO PERSONS OR TO AIRCRAFT OPERATIONS.
- ALL WORK TO BE PERFORMED WHICH IS CLOSE TO AN ACTIVE RUNWAY, TAXIWAY OR APRON SHALL BE PERFORMED WHEN THE RUNWAY, TAXIWAY OR APRON IS NOT IN USE. SUCH WORK SHALL BE ACCOMPLISHED ONLY WITH PRIOR PERMISSION FROM THE ENGINEER AND AIRPORT MANAGER. REQUESTED CLOSINGS SHALL BE DIRECTED TO THE ENGINEER AT LEAST 48 HOURS IN ADVANCE.
- THE FOLLOWING ARE CONSIDERED SAFETY PROBLEMS AND/OR HAZARDS:
 - TRENCHES, HOLES, OR EXCAVATION ON OR ADJACENT TO ANY OPEN RUNWAY OR IN RUNWAY OR TAXIWAY SAFETY AREAS.
 - UNMARKED/UNLIGHTED HOLES OR EXCAVATION IN ANY APRON, OPEN TAXIWAY, OPEN TAXILANE, OR RELATED SAFETY AREA.
 - MOUNDS OR PILES OF EARTH, CONSTRUCTION MATERIALS, TEMPORARY STRUCTURES, OR OTHER OBJECTS IN THE VICINITY OF ANY OPEN RUNWAY, TAXIWAY, TAXILANE, OR IN ANY RELATED SAFETY, APPROACH, OR DEPARTURE AREA.
 - VEHICLES OR EQUIPMENT, WHETHER OPERATING OR IDLE, ON ANY OPEN RUNWAY, TAXIWAY, TAXILANE OR IN ANY RELATED SAFETY, APPROACH, OR DEPARTURE AREA.
 - VEHICLES, EQUIPMENT, EXCAVATION, STOCKPILES, OR OTHER MATERIALS WHICH COULD INTERFERE WITH ELECTRONIC SIGNALS FROM RADIOS OR ELECTRONIC NAVIGATIONAL AIDS (NAVAIDS).
 - PAVEMENT DROP-OFFS - LIPS (EITHER PERMANENT OR TEMPORARY) WHICH COULD CAUSE DAMAGE TO AIRCRAFT IF CROSSED AT NORMAL OPERATING SPEEDS. THE NORMAL MAXIMUM DROP-OFF OR LIP IS 1-1/2 INCHES.
 - UNMARKED UTILITY, NAVAID, WEATHER SERVICE, RUNWAY LIGHTING, OR OTHER POWER OR SIGNAL CABLES THAT COULD BE DAMAGED DURING CONSTRUCTION.
 - OBJECTS, WHETHER OR NOT MARKED OR FLAGGED, OR ACTIVITIES ANYWHERE ON OR IN THE VICINITY OF THE AIRPORT WHICH COULD BE DISTRACTING, CONFUSING, OR ALARMING TO PILOTS DURING AIRCRAFT OPERATIONS.
 - UNFLAGGED/UNLIGHTED LOW VISIBILITY ITEMS SUCH AS TALL CRANES, DRILLS, AND THE LIKE ANYWHERE IN THE VICINITY OF ACTIVE RUNWAYS, OR IN ANY APPROACH OR DEPARTURE AREA.
 - THE CONTRACTOR SHALL CONDUCT ACTIVITIES SO AS NOT TO VIOLATE ANY SAFETY STANDARDS CONTAINED HEREIN. THE CONTRACTOR SHALL INSPECT ALL CONSTRUCTION AND STORAGE AREAS SO OFTEN AS NECESSARY AND PROMPTLY TAKE ALL STEPS NECESSARY TO PREVENT/REMEDY ANY UNSAFE OR POTENTIALLY UNSAFE CONDITIONS OR ACTIVITIES DISCOVERED.

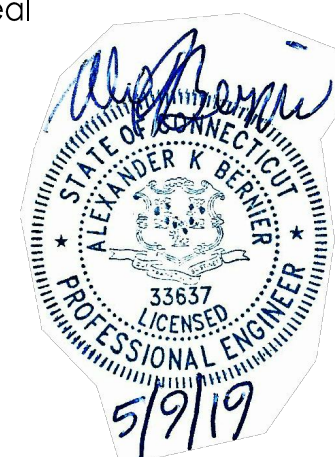
UNDERGROUND UTILITY NOTES

- THE LOCATIONS OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE CONSIDERED TO BE ONLY ESTIMATED LOCATIONS. ALL UTILITY LOCATIONS SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO COMMENCING CONSTRUCTION.
- PROTECTION OF EXISTING AIRPORT UNDERGROUND CABLES IS THE RESPONSIBILITY OF THE CONTRACTOR. THE LOCATION OF THESE UTILITIES MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF CABLES DAMAGED DUE TO CONTRACTOR'S OPERATIONS MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND SHALL BE AT THE CONTRACTOR'S EXPENSE. WHEN FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF AN FAA REPRESENTATIVE. THE FAA MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COST OF REPAIRS.
- AT LEAST TWO WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS IN AN AREA WHICH MAY INVOLVE UNDERGROUND UTILITY FACILITIES, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND THE OWNER OF EACH UNDERGROUND UTILITY FACILITY AFFECTED.
- AT LEAST TWO (2) FULL WORKING DAYS PRIOR TO COMMENCEMENT OF EXCAVATION, THE CONTRACTOR SHALL CONTACT DIGSAFE AT (888)-344-7233 FOR ASSISTANCE IN LOCATING UNDERGROUND UTILITIES.

SURVEY NOTES

- TOPOGRAPHIC SURVEY COMPLETED BY STANTEC CONSULTING SERVICES INCORPORATED, MARCH 2019, BASED ON THE SECONDARY AIRPORT CONTROL STATION, DESIGNATION, MMK B, WHOSE PUBLISHED COORDINATES ARE N41°30'37.86554" W072°49'51.02089" NAD 23 (2011) AND AN ELEVATION OF 99.0 FEET NAVD 88.

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Client/Project
MERIDEN MARKHAM MUNICIPAL AIRPORT

MID-BASE APRON REHABILITATION

MERIDEN, CONNECTICUT

Title

GENERAL NOTES

Project No. 195211190 Scale N.T.S.

Drawing No. C.001 Sheet 2 of 23 Revision 0

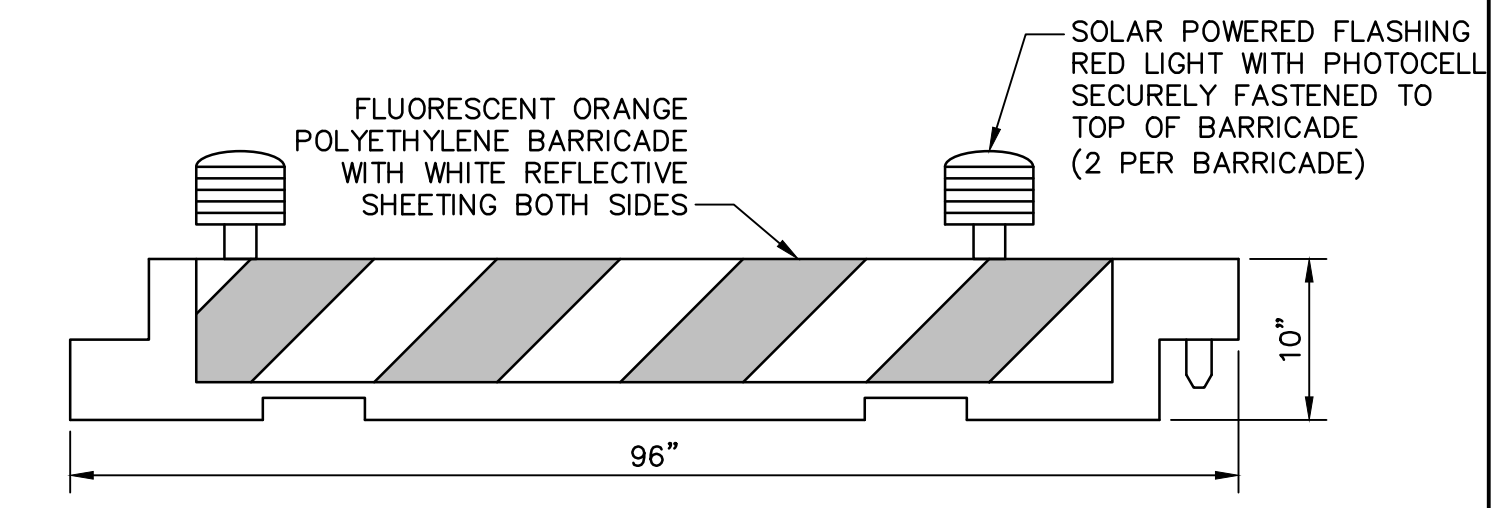
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LEGEND

	APPROXIMATE CITY/TOWN LINE
	CONSTRUCTION ACCESS ROUTE
	LOW PROFILE BARRICADE
	CONTRACTOR STAGING AREA
	PHASE 1 CONSTRUCTION
	TOFA TAXIWAY OBJECT FREE AREA
	TSA TAXIWAY SAFETY AREA
	RSA RUNWAY SAFETY AREA
	ROFA RUNWAY OBJECT FREE AREA

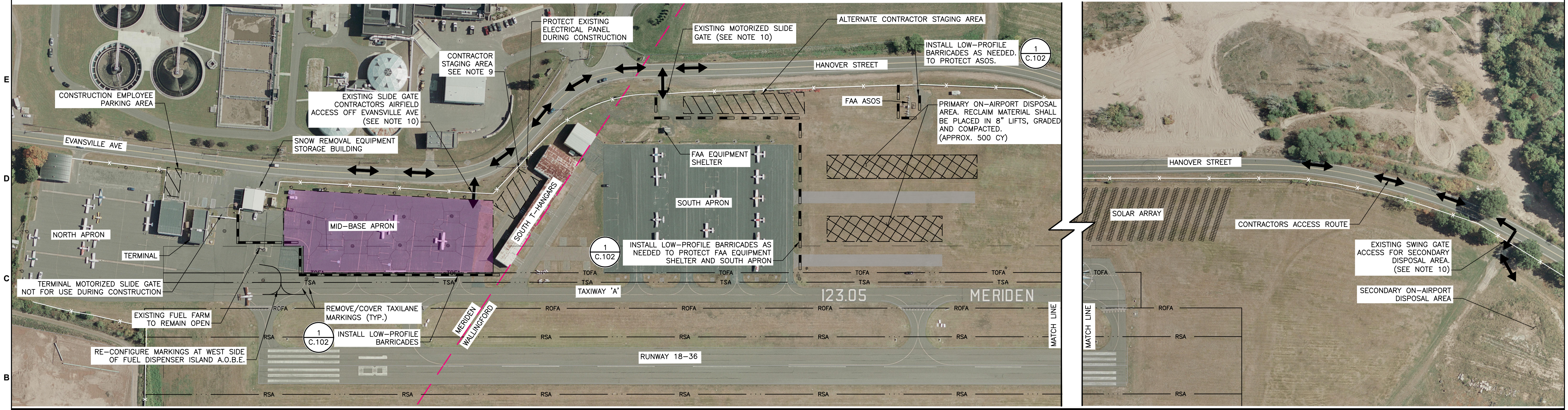
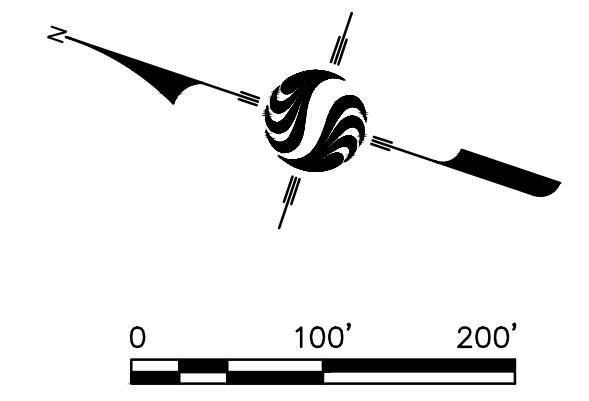
- I. PHASE 1 FULL-DEPTH RECLAMATION:**
- PHASE 1 CONSTRUCTION SHALL CONSIST OF THE RECLAMATION OF THE MID-BASE APRON UP TO THE TAXIWAY 'A' OBJECT FREE AREA, ALONG WITH THE CONSTRUCTION OF THE NEW DISPENSER ISLAND.
- II. CLOSURES:**
- MID-BASE APRON WILL BE CLOSED.
 - EAST SIDE OF THE EXISTING DISPENSER ISLAND WILL BE CLOSED.
- III. SCHEDULE:**
- UPON THE ISSUANCE OF A NOTICE TO PROCEED, THE TOTAL CONSTRUCTION DURATION FOR PHASE 1 WILL BE 30 CALENDAR DAYS.
 - WORK SHALL BE PERFORMED DURING DAYTIME HOURS.
 - PHASE 1 WORK SHALL BE COMPLETED BEFORE MOVING TO PHASE 2 AND 3.
- IV. DESCRIPTION OF MAJOR WORK EFFORTS:**
- FURNISH AND INSTALL EROSION AND SEDIMENT CONTROL DEVICES.
 - FURNISH AND INSTALL TRAFFIC CONTROL DEVICES (ie. CHANNELIZER CONES, TEMPORARY STRIPING, BARRICADES, ETC.).
 - REMOVE/COVER TAXILANE MARKINGS TO LIMIT OF PHASE 1 CONSTRUCTION. RE-CONFIGURE MARKINGS AT WEST SIDE OF FUEL DISPENSER ISLAND TO MAINTAIN FUELING OPERATIONS DURING PHASE 1.
 - DEMOLISH EXISTING TIE DOWNS, DRAINAGE PIPE AND ANY MISCELLANEOUS ITEMS.
 - RECLAIM PAVEMENT FULL-DEPTH. DISPOSE OF EXCESS MATERIAL ON-SITE.
 - EXCAVATE FOR APRON EXPANSION.
 - GRADE AND COMPACT RECLAIMED BASE COURSE.
 - ADJUST DRYWELL FRAMES AND GRATES.
 - INSTALL PROPOSED DISPENSER ISLAND.
 - PLACE ONE 1.5" LIFT OF HOT MIX ASPHALT (HMA).

- CSPP NOTES:**
- SEE DRAWING C.001 FOR GENERAL NOTES.
 - THIS PLAN HAS BEEN APPROVED BY THE MERIDEN-MARKHAM MUNICIPAL AIRPORT (MMK). NO CHANGE OR MODIFICATION TO THIS PLAN WILL BE PERMITTED WITHOUT ADVANCED WRITTEN APPROVAL BY THE ENGINEER.
 - SCHEDULING OF ALL WORK SHALL BE COORDINATED IN WRITING WITH THE AIRPORT MANAGER THROUGH THE ENGINEER A MINIMUM OF 48 HOURS IN ADVANCE.
 - THERE IS NO SEPARATE PAY ITEM TO PROVIDE AND MAINTAIN TRAFFIC CONTROL DEVICES AND MEASURES INCLUDING BUT NOT LIMITED TO LIGHTED BARRICADES, LIGHTED CHANNELIZER CONES, SIGNING, TEMPORARY STRIPING, ETC. ALL WORK ASSOCIATED WITH THESE MEASURES IS CONSIDERED INCIDENTAL TO THE PROJECT.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR MODIFICATIONS AND RESTORATION OF THE EXISTING GATE AND FENCE LINE (IF NECESSARY) TO ALLOW CONSTRUCTION VEHICLE ACCESS TO THE WORK AREA AT NO ADDITIONAL EXPENSE TO THE OWNER.
 - THE CONTRACTOR SHALL CONTINUOUSLY COORDINATE WITH THE ENGINEER AND THE AIRPORT MANAGER REGARDING DAILY CONSTRUCTION ACTIVITIES/OPERATIONS.
 - RESTORATION OF TURF AREAS AFFECTED BY CONSTRUCTION ACTIVITIES IS CONSIDERED INCIDENTAL TO ALL OTHER PAY ITEMS.
 - THE CONTRACTOR SHALL DISPOSE EXCESS RECLAIM MATERIAL ON-SITE ONLY AFTER UTILIZING THE MATERIAL AS AGGREGATE BASE COURSE FOR THE APRON EXPANSION. THE ON-SITE DISPOSAL AREAS ARE SHOWN ON THE PLANS AND QUANTITIES OF DISPOSAL SHALL BE DETERMINED BY THE RESIDENT ENGINEER.
 - THE CONTRACTOR SHALL PROTECT THE EXISTING T-HANGAR BUILDING DURING CONSTRUCTION AFTER TENANTS ARE TEMPORARILY RELOCATED. ANY DAMAGE CAUSED BY CONSTRUCTION OPERATIONS SHALL BE REPAIRED/REPLACED AT NO COST TO THE AIRPORT AND/OR TENANTS.
 - THE CONTRACTOR SHALL PROVIDE APPROPRIATE HIGHWAY WARNING SIGNS ALONG EVANSVILLE AVE. AND HANDOVER STREET DURING CONSTRUCTION, MEETING THE REQUIREMENTS OF USDOT FHA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION. THE RESIDENT ENGINEER SHALL APPROVE THE SIGNING PACKAGE PRIOR TO INSTALLATION. THIS WORK IS CONSIDERED INCIDENTAL TO THE PROJECT.



- SAFETY BARRICADE NOTES:**
- THERE IS NO SEPARATE PAY ITEM FOR LIGHTED SAFETY BARRICADES AND THIS WORK IS CONSIDERED INCIDENTAL TO THE PROJECT.
 - THE LIGHTED SAFETY BARRICADES SHALL BE USED WHEN A WORK AREA IS DIRECTLY ADJACENT TO AN ACTIVE RUNWAY, TAXIWAY OR TAXILANE TO KEEP AIRCRAFT AND VEHICLES FROM TRAVELING INTO A WORK AREA.
 - BARRICADES SHALL BE INTERCONNECTED ON AIRFIELD PAVEMENTS OR AS IDENTIFIED ON PLANS.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR RELOCATING AND/OR PROVIDING ADDITIONAL BARRICADES AROUND THE SITE AS NECESSARY TO ACCOMMODATE THE PROPOSED WORK.
 - THE CONTRACTOR SHALL ENSURE THE CONTINUOUS OPERATION OF ALL LIGHTS FOR THE DURATION OF THE PROJECT. LIGHTS AND/OR BATTERIES SHALL BE REPLACED AS NEEDED.
 - THE BARRICADES SHALL BE FILLED WITH WATER OR OTHERWISE SECURED TO PREVENT MOVEMENT.

1 TYPICAL LOW-PROFILE BARRICADE DETAIL
NOT TO SCALE



Revision	By	Appd.	MM.DD.YY	Issued	By	Appd.	YY.MM.DD

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MERIDEN MARKHAM MUNICIPAL AIRPORT
MID-BASE APRON REHABILITATION
MERIDEN, CONNECTICUT

File Name: 195211190
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Title
CONSTRUCTION SAFETY AND PHASING PLAN
PHASE 1

Project No. 195211190
Drawing No. C.002

Scale 1" = 100'
Sheet 3 of 23

Revision 0

LEGEND

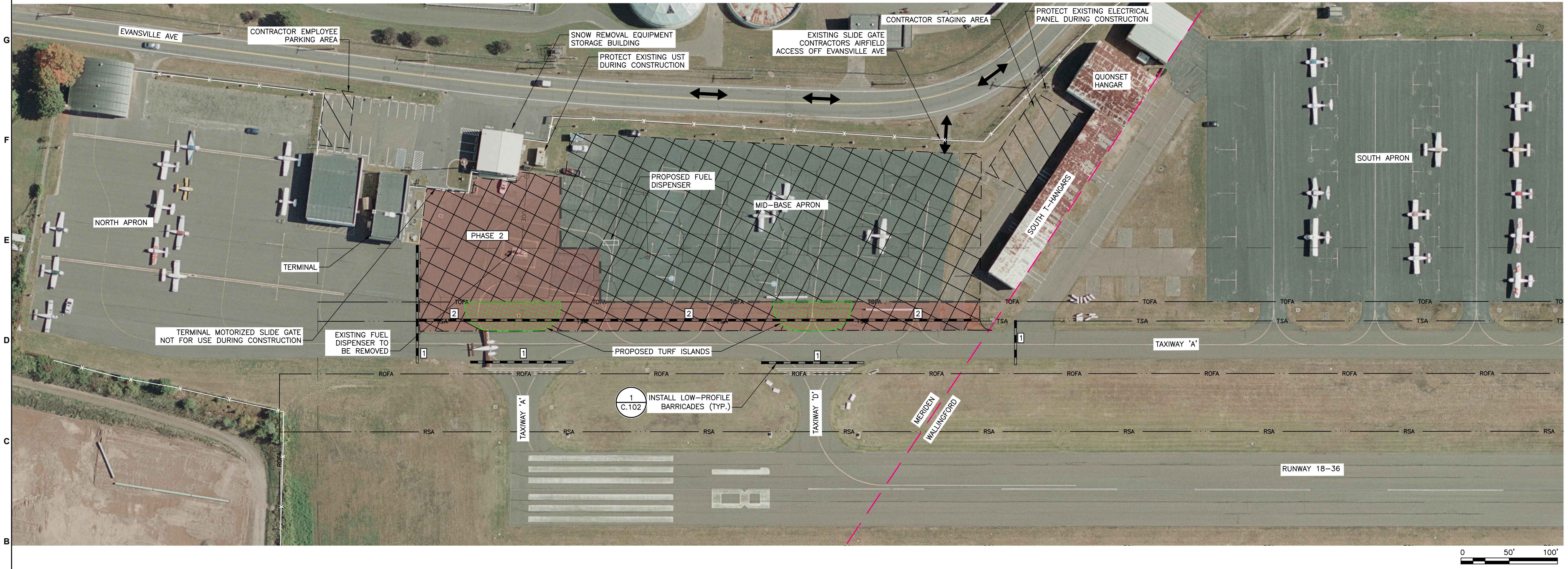
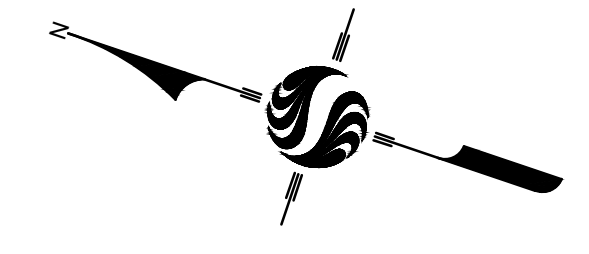
	APPROXIMATE CITY/TOWN LINE
	CONSTRUCTION ACCESS ROUTE
	LOW-PROFILE BARRICADE
	CONTRACTOR STAGING AREA
	PHASE 2 CONSTRUCTION
	PHASE 3 CONSTRUCTION
	TOFA TAXIWAY OBJECT FREE AREA
	TSA TAXIWAY SAFETY AREA
	RSA RUNWAY SAFETY AREA
	ROFA RUNWAY OBJECT FREE AREA

- I. PHASE 2 COLD MILLING:**
- PHASE 2 SHALL CONSIST OF COLD MILLING THE EXISTING PAVEMENT TO A DEPTH OF 1.5", THE DEMOLITION OF THE EXISTING FUEL DISPENSER AND THE COMMISSIONING OF THE NEW FUEL DISPENSER ISLAND.
- II. CLOSURES:**
- MID-BASE APRON
 - TAXIWAY 'A' NORTH - RESTRICTED ACCESS DURING DAYTIME CONSTRUCTION
 - FUEL DISPENSER ISLAND
 - NORTH APRON - RESTRICTED ACCESS DURING DAYTIME CONSTRUCTION
 - TERMINAL SLIDE GATE - RESTRICTED ACCESS DURING DAYTIME CONSTRUCTION
- III. SCHEDULE:**
- PHASE 2 CONSTRUCTION DURATION IS 20 CALENDAR DAYS.
 - WORK SHALL BE PERFORMED DURING DAYTIME HOURS.
 - PHASE 2 WORK SHALL BE COMPLETED BEFORE MOVING TO PHASE 3.
- IV. DESCRIPTION OF MAJOR WORK EFFORTS:**
- MILL 1.5" HMA, REMOVE AND STOCKPILE MILLINGS AT SECONDARY ON-AIRPORT DISPOSAL AREA.
 - DEMOLISH AND REMOVE EXISTING FUEL DISPENSER ISLAND AND MISCELLANEOUS EQUIPMENT.
 - EXCAVATE AND/OR MILL FULL DEPTH FOR TURF ISLANDS.
 - INSTALL TAXIWAY EDGE LIGHTS AND CONDUIT.
 - COMMISSION PROPOSED DISPENSER.
 - FURNISH AND PLACE OFF-SITE TOPSOIL AND HYDROSEED.

- I. PHASE 3 FINAL HMA OVERLAY (ENTIRE APRON):**
- PHASE 3 SHALL CONSIST OF A SINGLE 1.5" LIFT OF HMA PAVEMENT OVER THE ENTIRE WORK AREA (PHASE 1 AND PHASE 2).
- II. CLOSURES:**
- MID-BASE APRON
 - TAXIWAY 'A' NORTH
 - FUEL DISPENSER ISLAND
 - TERMINAL SLIDE GATE
- III. SCHEDULE:**
- PHASE 3 CONSTRUCTION DURATION IS 10 CALENDAR DAYS.
 - WORK SHALL BE PERFORMED DURING DAYTIME HOURS.
- IV. DESCRIPTION OF MAJOR WORK EFFORTS:**
- APPLY TACK COAT.
 - PLACE 1.5" LIFT OF HOT MIX ASPHALT (HMA).
 - FURNISH AND INSTALL TIE-DOWN ANCHORS.
 - PLACE FINAL PAVEMENT MARKINGS.
 - CLEAN AND RESTORE WORK AREAS TO A CONDITION APPROVED BY THE AIRPORT MANAGER AND ENGINEER. ANY DAMAGE TO PAVED DRIVEWAYS/APRONS AS A RESULT OF CONSTRUCTION OPERATIONS SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE AIRPORT.
 - OPEN MID-BASE APRON AND FUEL DISPENSER ISLAND.

- NOTES:**
- SEE DRAWING C.001 FOR GENERAL NOTES.
 - SEE DRAWING C.002 FOR ADDITIONAL CSPP NOTES.

- TAXIWAY A CLOSURE REQUIREMENTS:**
- THE NORTHERN END OF TAXIWAY A WILL NEED TO BE CLOSED DAILY WHEN CONSTRUCTION ACTIVITIES OCCUR WITHIN THE TSA AND TOFA.
 - THE AIRPORT MANAGER SHALL ISSUE THE APPROPRIATE NOTAM AND LOW-PROFILE BARRICADES SHALL BE INSTALLED AT THE LOCATIONS IDENTIFIED WITH A [1].
 - AT THE END OF EACH WORK DAY TAXIWAY A SHALL BE OPENED TO AIRCRAFT OPERATIONS AS FOLLOWS:
 - REMOVE LOW-PROFILE BARRICADES IDENTIFIED WITH A [1].
 - ISSUE APPROPRIATE NOTAM FOR CONSTRUCTION ACTIVITIES ADJACENT TO A TAXIWAY.
 - INSTALL LOW-PROFILE BARRICADES ALONG THE TSA IDENTIFIED ON THE PLAN WITH A [2].
 - ALL EXCAVATIONS WITHIN THE TSA AND TOFA SHALL BE BROUGHT TO WITHIN 1 1/2" OF FINISHED GRADE.
 - NO EQUIPMENT OR STOCKPILES SHALL BE ALLOWED IN THE TSA AND TOFA.



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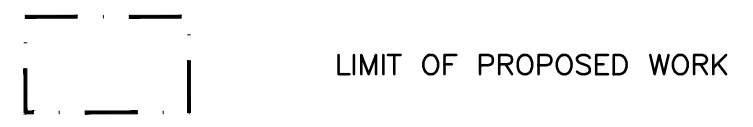
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Client/Project	MERIDEN MARKHAM MUNICIPAL AIRPORT
	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
CAM Dwn.	PVE Chkd.
CAM Dsgn.	2019.05.09 YY.MM.DD

Title		
CONSTRUCTION SAFETY AND PHASING PLAN PHASES 2 & 3		
Project No.	Scale	
195211190	1" = 50'	
Drawing No.	Sheet	Revision
C.003	4 of 23	0

LEGEND

- TOFA — TAXIWAY OBJECT FREE AREA
- TSA — TAXIWAY SAFETY AREA
- ROFA — RUNWAY OBJECT FREE AREA
- ⊕ — SURVEY BENCHMARK



- 99.0 — EXISTING MAJOR CONTOUR
- 99.2 — EXISTING MINOR CONTOUR
- ⊕ — EXISTING ELECTRICAL HANDHOLE
- ⊕ — EXISTING DRY WELL W/GRATE
- ⊕ — EXISTING AIRFIELD LIGHT
- ▨ — EXISTING AIRFIELD PAVMENTS

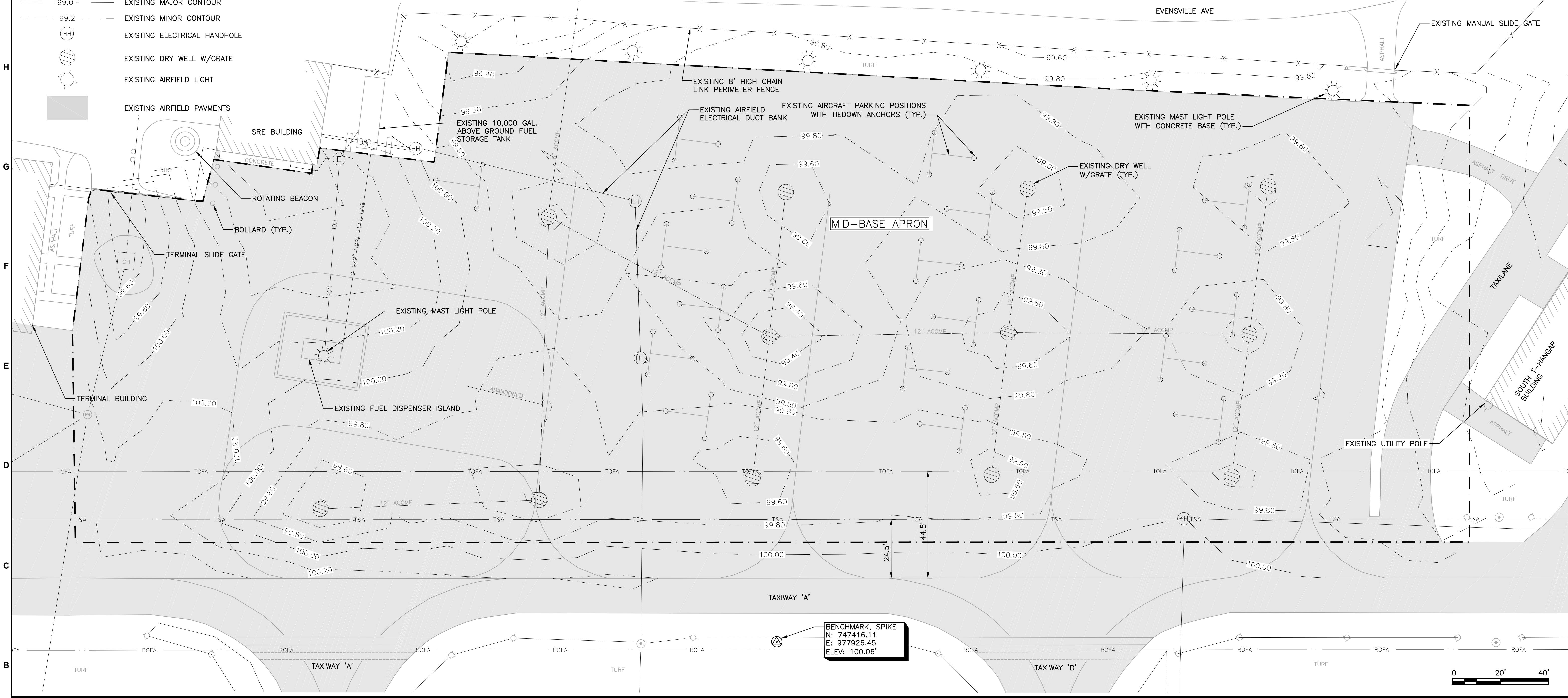
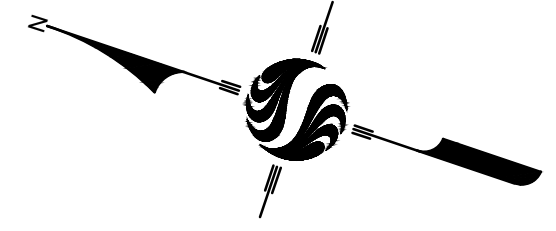
NOTES:

1. REFER TO UNDERGROUND UTILITY NOTES OF DRAWING C.001.
2. ALL EXISTING UTILITIES AND STRUCTURES NOT PART OF THIS PROJECT ARE TO REMAIN UNDISTURBED. ANY DISTURBED OR DAMAGED AIRPORT, CITY OR FAA PROPERTY SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE OWNER.
3. THE CONTRACTOR SHALL UTILIZE A PROFESSIONAL LAND SURVEYOR, REGISTERED IN THE STATE OF CONNECTICUT FOR ALL SURVEY AND LAYOUT WORK.

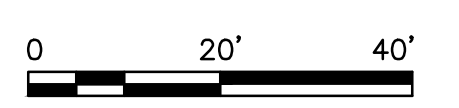
NATIONAL GEODETIC SURVEY MONUMENTS:

MMK PACS A
ELEV: 101.20'
N: 747672.84
E: 977527.38

MMK SACS B
ELEV: 99.00'
N: 746751.78
E: 977859.13



BENCHMARK, SPIKE
N: 747416.11
E: 977926.45
ELEV: 100.06'



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	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
CAM Dwn.	PVE Chkd.
CAM Dsgn.	2019.05.09 YY.MM.DD

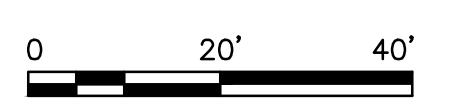
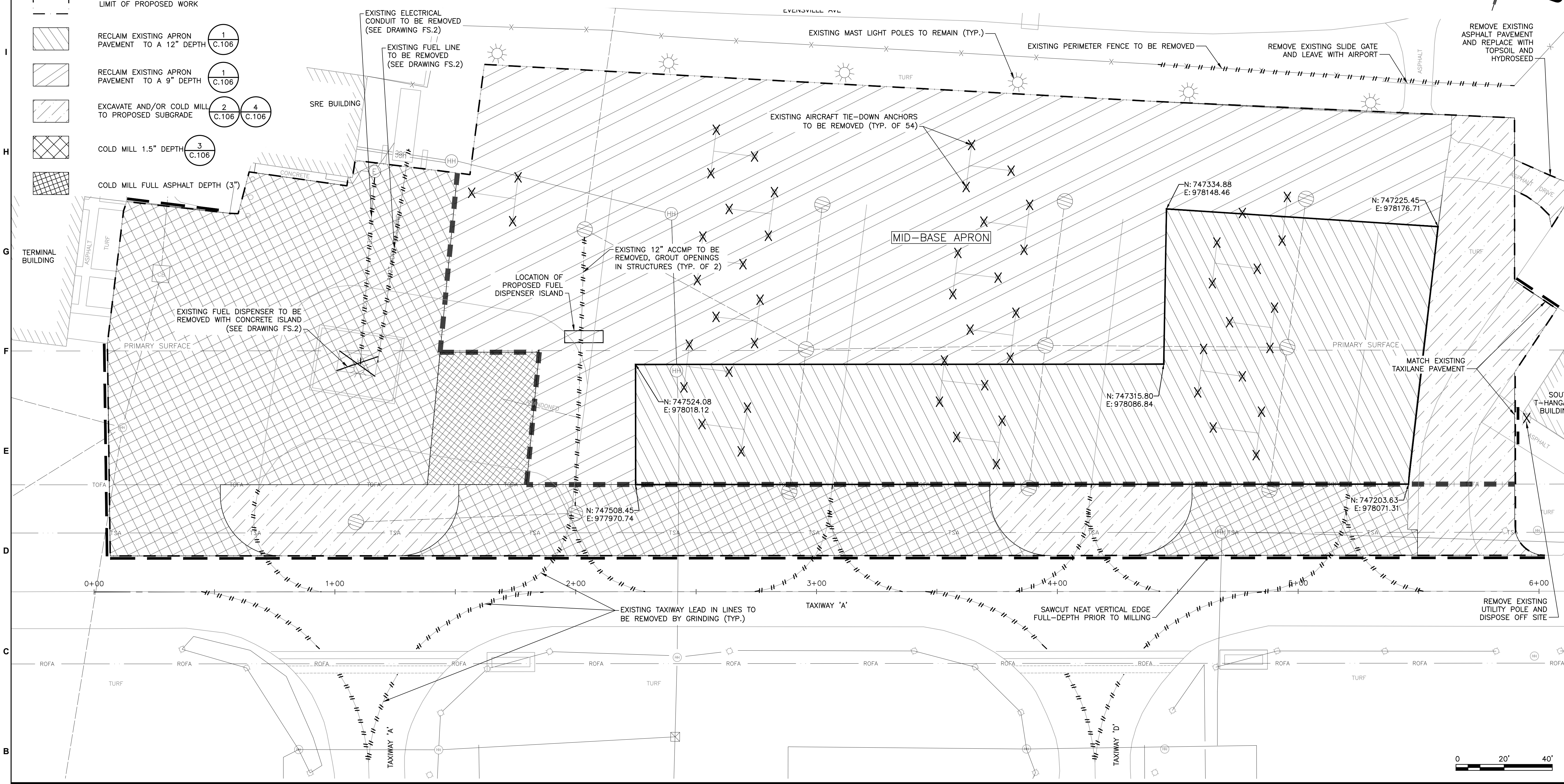
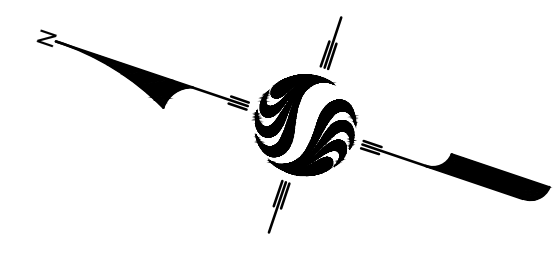
Title		
EXISTING CONDITIONS PLAN		
Project No.	Scale	
195211190	1" = 20'	
Drawing No.	Sheet	Revision
C.101	5 of 23	0

LEGEND

- ▬▬▬▬ PHASE LIMIT
- ▬▬▬▬ FULL-DEPTH SAWCUT
- X DEMOLISH OBJECT
- ||||| DEMOLISH AS LENGTH INDICATES
- ▭ LIMIT OF PROPOSED WORK
- ▨ RECLAIM EXISTING APRON PAVEMENT TO A 12" DEPTH (C.106)
- ▧ RECLAIM EXISTING APRON PAVEMENT TO A 9" DEPTH (C.106)
- ▩ EXCAVATE AND/OR COLD MILL TO PROPOSED SUBGRADE (C.106)
- ▩ COLD MILL 1.5" DEPTH (C.106)
- ▩ COLD MILL FULL ASPHALT DEPTH (3") (C.106)

NOTES:

1. THE EXISTING BASE PLAN MAY NOT SHOW ALL EXISTING SURFACE AND SUBSURFACE STRUCTURES AND UTILITIES SUCH AS, BUT NOT LIMITED TO, CLEAN OUTS, VALVE BOXES, GROUND RECEPTACLES, MANHOLES, STORM DRAINS, DUCT BANKS, CATCH BASINS, ELECTRICAL CABLES, ETC. THE CONTRACTOR SHALL IDENTIFY ALL EXISTING STRUCTURES AND UTILITIES PRIOR TO THE START OF SITE WORK OPERATIONS. ANY DAMAGE TO THESE STRUCTURES WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIX AND/OR REPLACE IMMEDIATELY AT NO ADDITIONAL COST TO THE OWNER. REFER TO NOTES ON THE EXISTING CONDITIONS PLAN, DRAWING C.101, FOR REQUIREMENTS.



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Client/Project	MERIDEN MARKHAM MUNICIPAL AIRPORT
	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
CAM Dwn.	PVE Chkd.
CAM Dsgn.	2019.05.09 YY.MM.DD

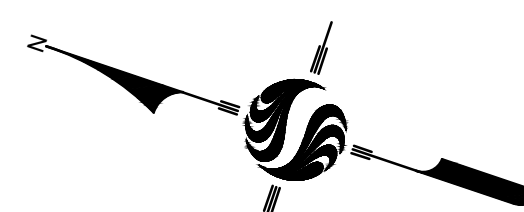
Title		DEMOLITION PLAN	
Project No.	Scale	1" = 20'	
195211190			
Drawing No.	Sheet	Revision	
C.103	7 of 23	0	

LEGEND

- PHASE LIMIT
- 3" HMA ON 6" RECYCLED BASE COURSE (1) C.106
- FULL-DEPTH APRON CONSTRUCTION 3" HMA ON 6" RECYCLED BASE COURSE (2) C.106
- 1.5" HMA OVERLAY (3) C.106
- TURF ISLAND (4) C.106
- BITUMINOUS JOINT SAW AND SEAL (6) C.106
- POINT I.D. #, SEE POINT TABLE

NOTES:

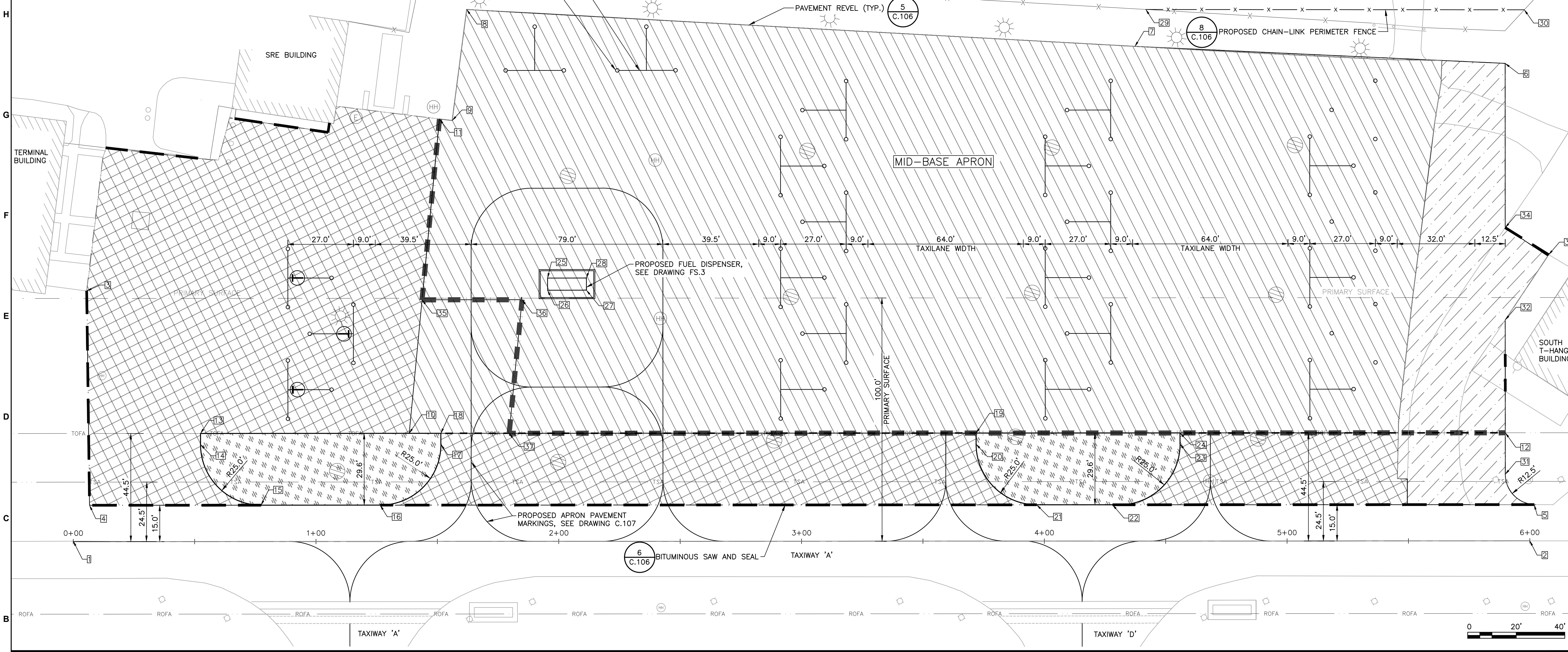
1. THE EXISTING BASE PLAN MAY NOT SHOW ALL EXISTING SURFACE AND SUBSURFACE STRUCTURES AND UTILITIES SUCH AS, BUT NOT LIMITED TO, CLEAN OUTS, VALVE BOXES, GROUND RECEPTACLES, MANHOLES, STORM DRAINS, DUCT BANKS, CATCH BASINS, ELECTRICAL CABLES, ETC. THE CONTRACTOR SHALL IDENTIFY ALL EXISTING STRUCTURES AND UTILITIES PRIOR TO THE START OF SITE WORK OPERATIONS. ANY DAMAGE TO THESE STRUCTURES WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIX AND/OR REPLACE IMMEDIATELY AT NO ADDITIONAL COST TO THE OWNER. REFER TO NOTES ON THE EXISTING CONDITIONS PLAN, DRAWING C.101, FOR REQUIREMENTS.



Point #	Northing	Easting	Description
1	747708.13	977858.19	TW A ALIGNMENT
2	747138.27	978045.98	TW A ALIGNMENT
3	747735.12	977957.77	LIMIT OF WORK
4	747706.38	977874.31	LIMIT OF WORK
5	747141.24	978060.75	LIMIT OF WORK
6	747209.45	978229.58	LIMIT OF WORK
7	747356.46	978188.51	LIMIT OF WORK
8	747622.70	978116.85	LIMIT OF WORK
9	747614.07	978071.52	LIMIT OF WORK
10	747590.59	977943.64	PHASE LIMIT
11	747619.24	978070.50	PHASE LIMIT
12	747161.79	978085.11	PHASE LIMIT
13	747672.22	977916.71	TURF ISLAND

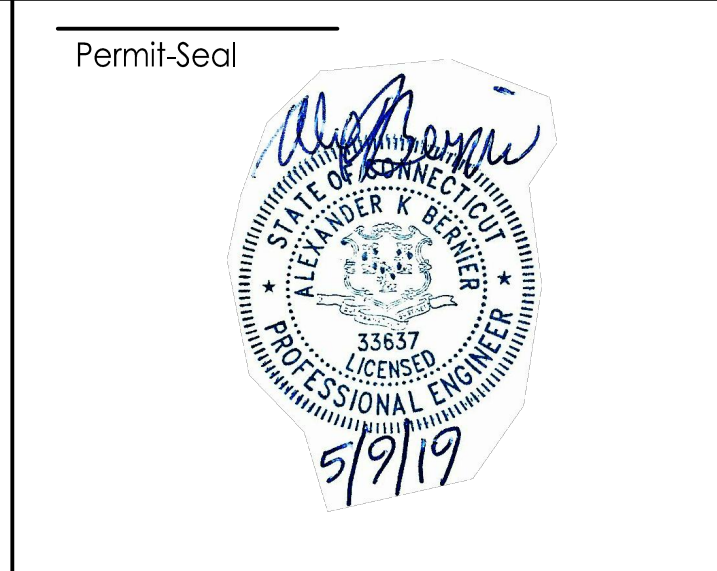
Point #	Northing	Easting	Description
14	747670.80	977912.38	TURF ISLAND
15	747639.28	977896.45	TURF ISLAND
16	747592.69	977911.82	TURF ISLAND
17	747576.78	977943.39	TURF ISLAND
18	747578.21	977947.73	TURF ISLAND
19	747368.81	978016.81	TURF ISLAND
20	747367.38	978012.48	TURF ISLAND
21	747335.87	977996.55	TURF ISLAND
22	747306.17	978006.35	TURF ISLAND
23	747287.61	978038.80	TURF ISLAND
24	747289.04	978043.13	TURF ISLAND
25	747556.45	978022.21	FUEL DISPENSER ISLAND
26	747554.88	978017.46	FUEL DISPENSER ISLAND

Point #	Northing	Easting	Description
27	747539.69	978022.47	FUEL DISPENSER ISLAND
28	747541.26	978027.22	FUEL DISPENSER ISLAND
29	747356.95	978204.36	SECURITY FENCE
30	747208.85	978253.00	SECURITY FENCE
31	747156.44	978068.89	LIMIT OF WORK
32	747176.35	978129.24	LIMIT OF WORK
33	747167.74	978160.35	LIMIT OF WORK
34	747188.26	978165.32	LIMIT OF WORK
35	747602.75	977997.52	PHASE LIMIT
36	747563.64	978010.45	PHASE LIMIT
37	747551.47	977956.55	PHASE LIMIT



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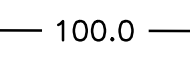

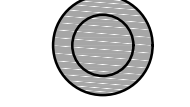
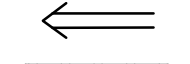



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	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
CAM Dwn.	PVE Chkd.
CAM Dsgn.	2019.05.09
	YY.MM.DD

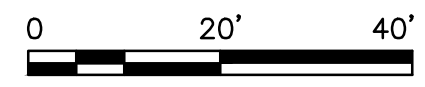
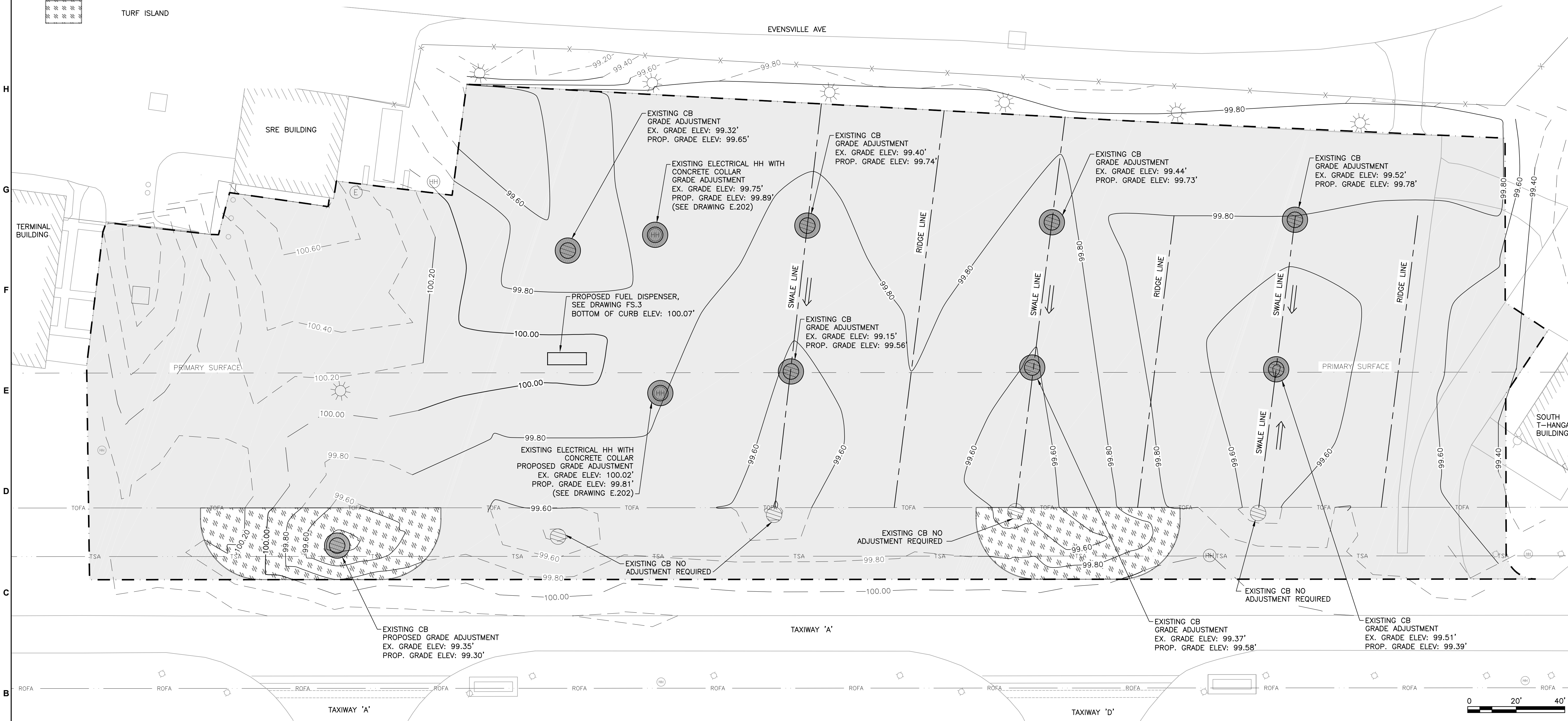
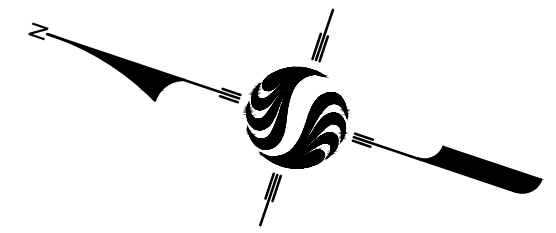
Title	APRON LAYOUT PLAN		
Project No.	195211190	Scale	1" = 20'
Drawing No.	C.104	Sheet	8 of 23
		Revision	0

LEGEND

-  LIMIT OF PROPOSED WORK
-  99.0 - EXISTING MAJOR CONTOUR
-  99.2 - EXISTING MINOR CONTOUR
-  100.0 - PROPOSED MAJOR CONTOUR
-  99.8 - PROPOSED MINOR CONTOUR
-  GRADE ADJUSTMENT TO EX. STRUCTURE 7
C.106
-  FLOW DIRECTION
-  TURF ISLAND

NOTES:

1. THE EXISTING BASE PLAN MAY NOT SHOW ALL EXISTING SURFACE AND SUBSURFACE STRUCTURES AND UTILITIES SUCH AS, BUT NOT LIMITED TO, CLEAN OUTS, VALVE BOXES, GROUND RECEPTACLES, MANHOLES, STORM DRAINS, DUCT BANKS, CATCH BASINS, ELECTRICAL CABLES, ETC. THE CONTRACTOR SHALL IDENTIFY ALL EXISTING STRUCTURES AND UTILITIES PRIOR TO THE START OF SITE WORK OPERATIONS. ANY DAMAGE TO THESE STRUCTURES WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIX AND/OR REPLACE IMMEDIATELY AT NO ADDITIONAL COST TO THE OWNER. REFER TO NOTES ON THE EXISTING CONDITIONS PLAN, DRAWING C.101, FOR REQUIREMENTS.



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Issued	By	Appd.	YY.MM.DD

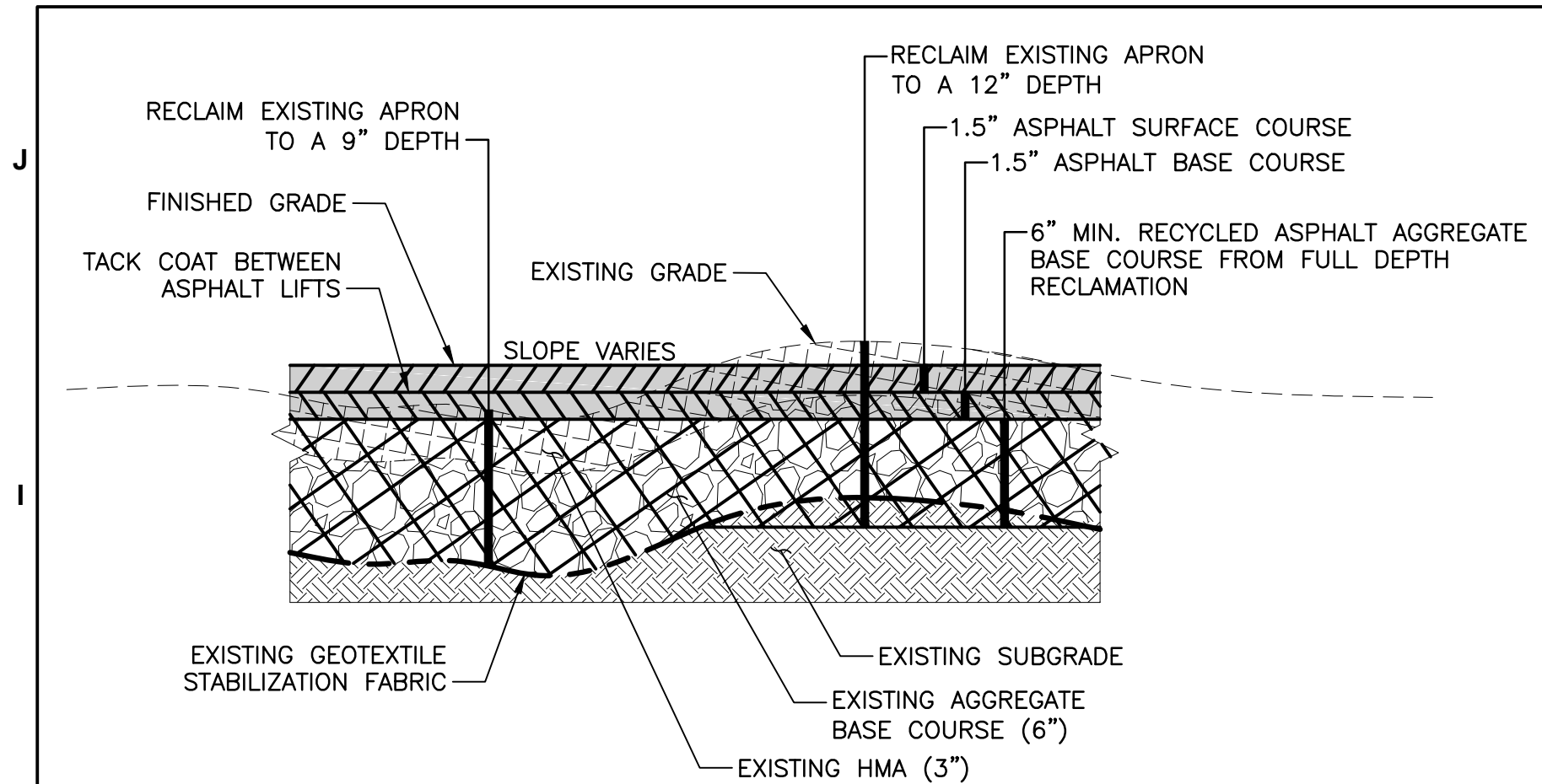
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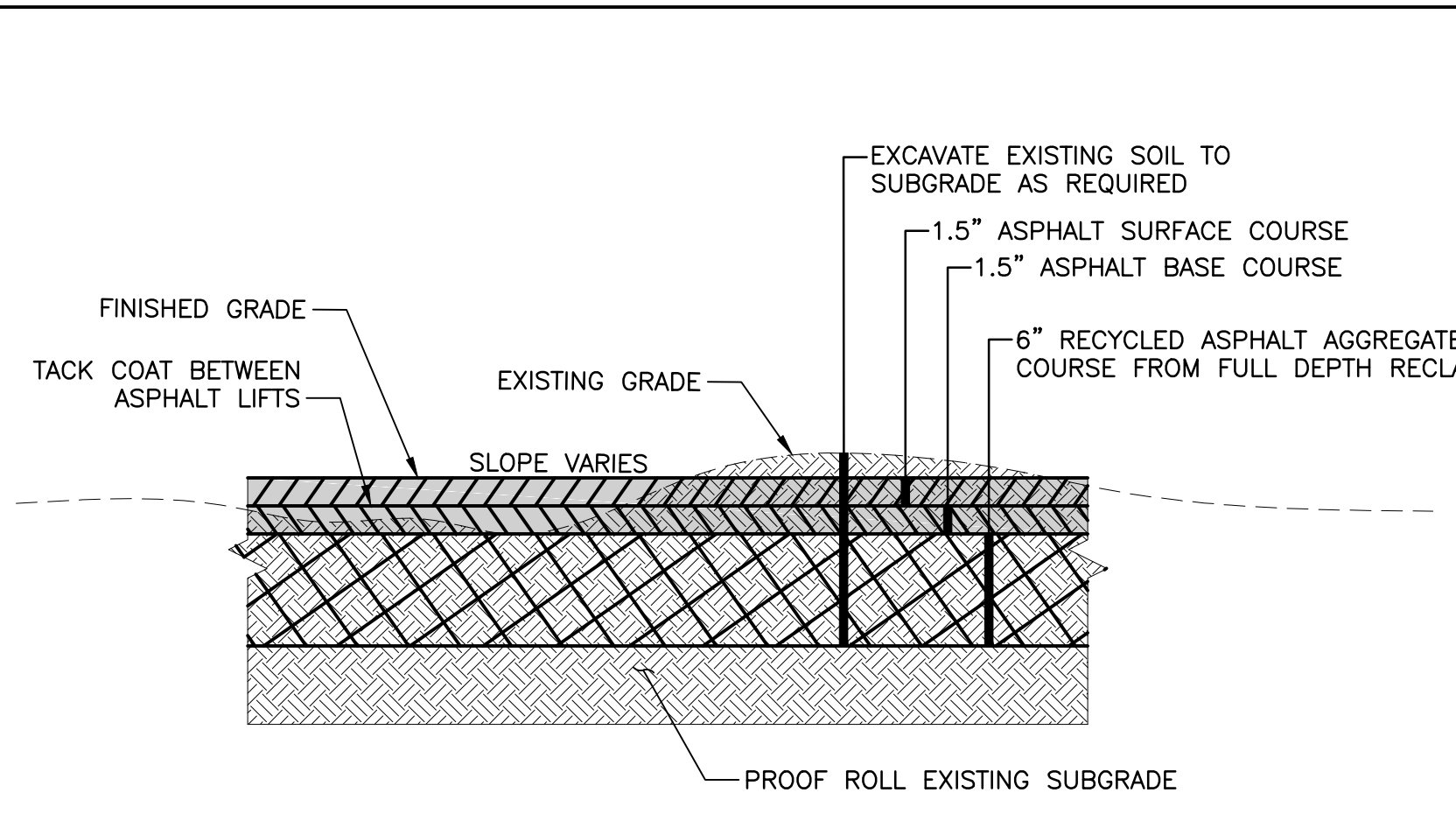
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	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
CAM Dwn.	PVE Chkd.
CAM Dsgn.	2019.05.09 YY.MM.DD

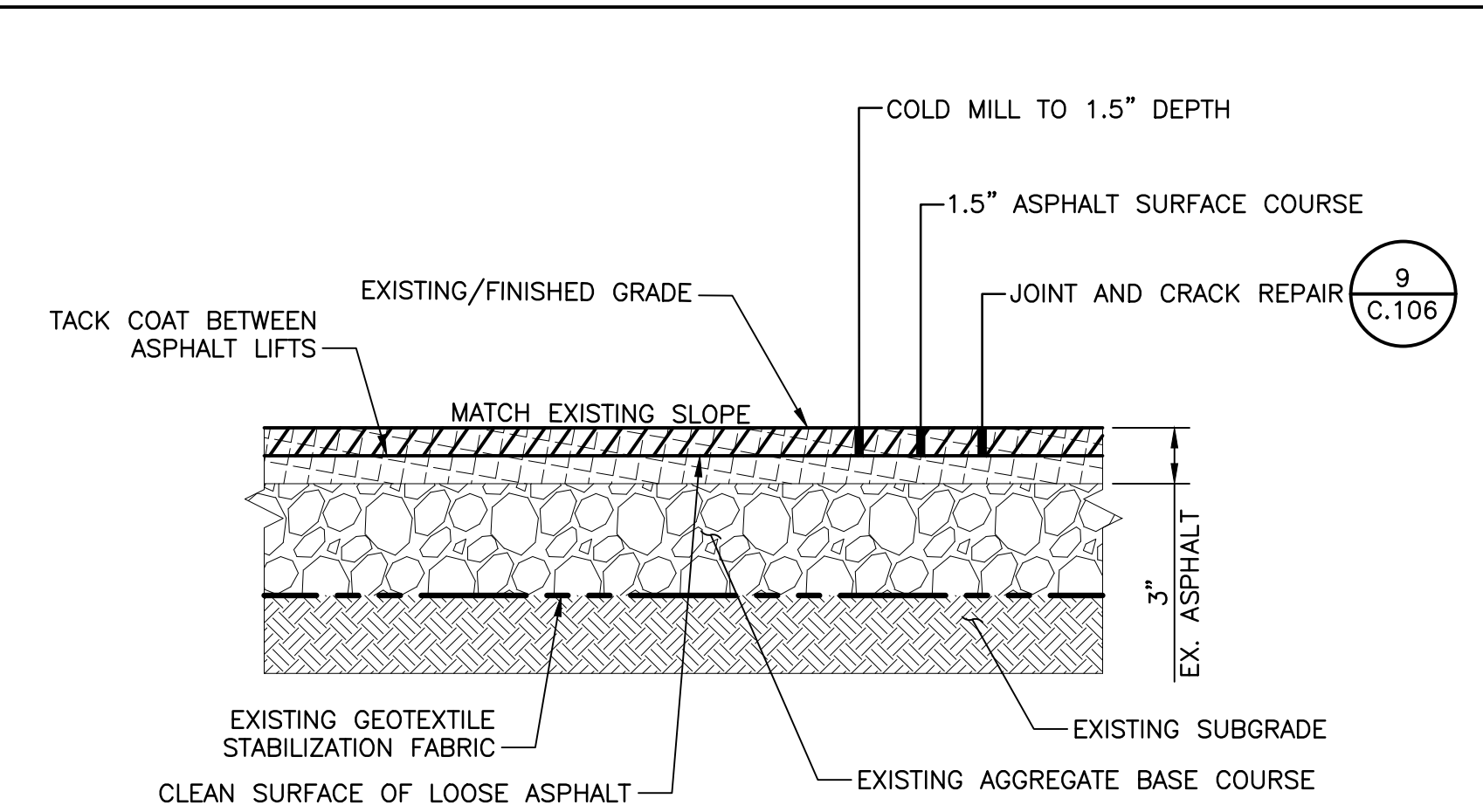
Title		
APRON GRADING PLAN		
Project No.	Scale	
195211190	1" = 20'	
Drawing No.	Sheet	Revision
C.105	9 of 23	0



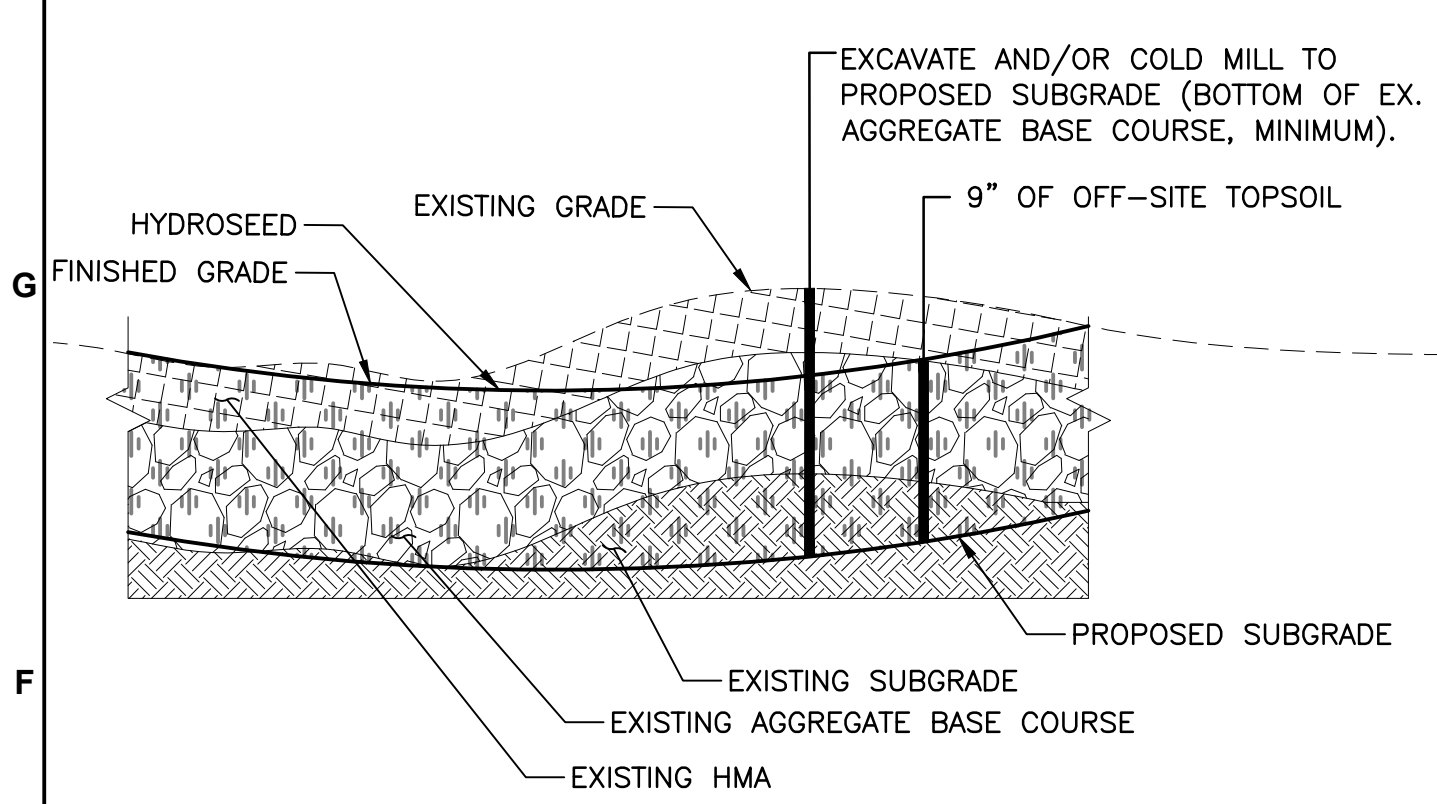
1 TYPICAL FULL-DEPTH APRON RECLAMATION SECTION
NOT TO SCALE



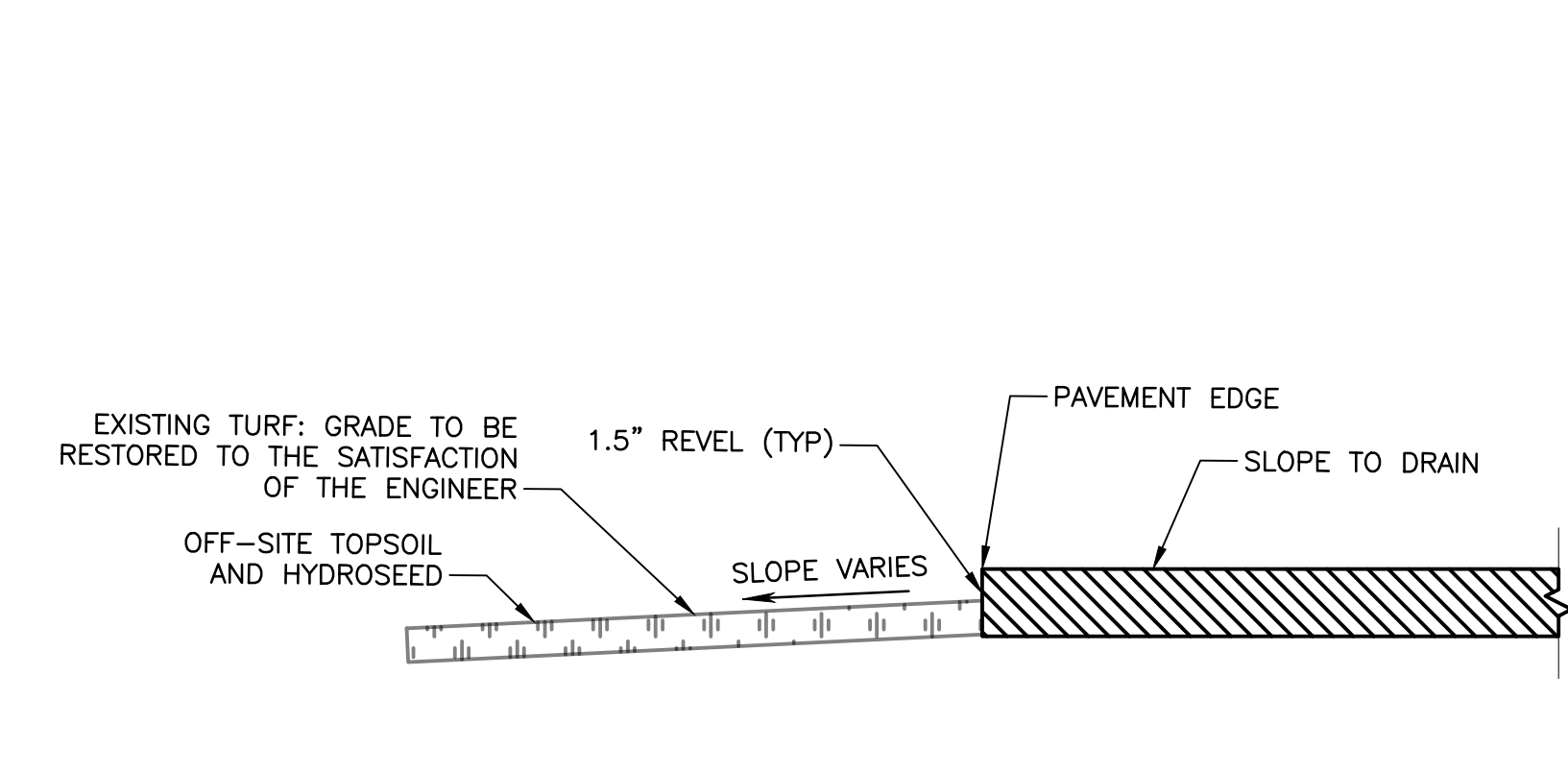
2 TYPICAL FULL-DEPTH APRON CONSTRUCTION SECTION
NOT TO SCALE



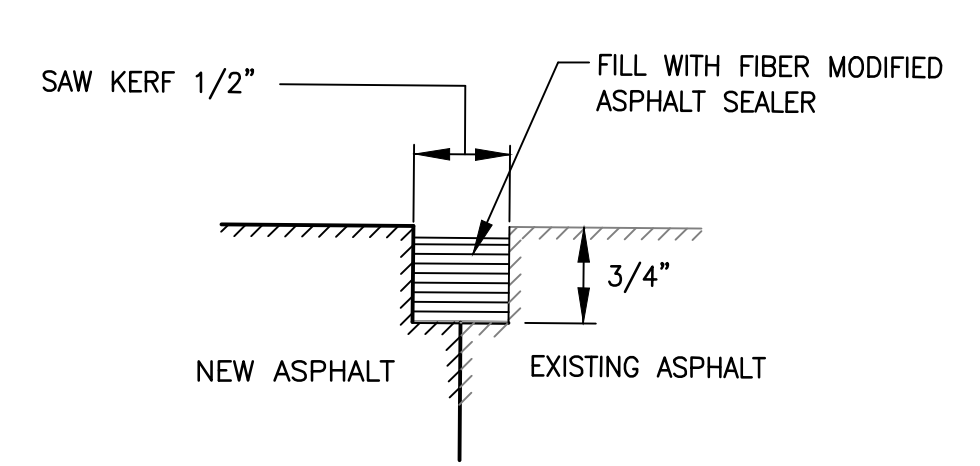
3 TYPICAL 1.5" COLD MILL AND OVERLAY SECTION
NOT TO SCALE



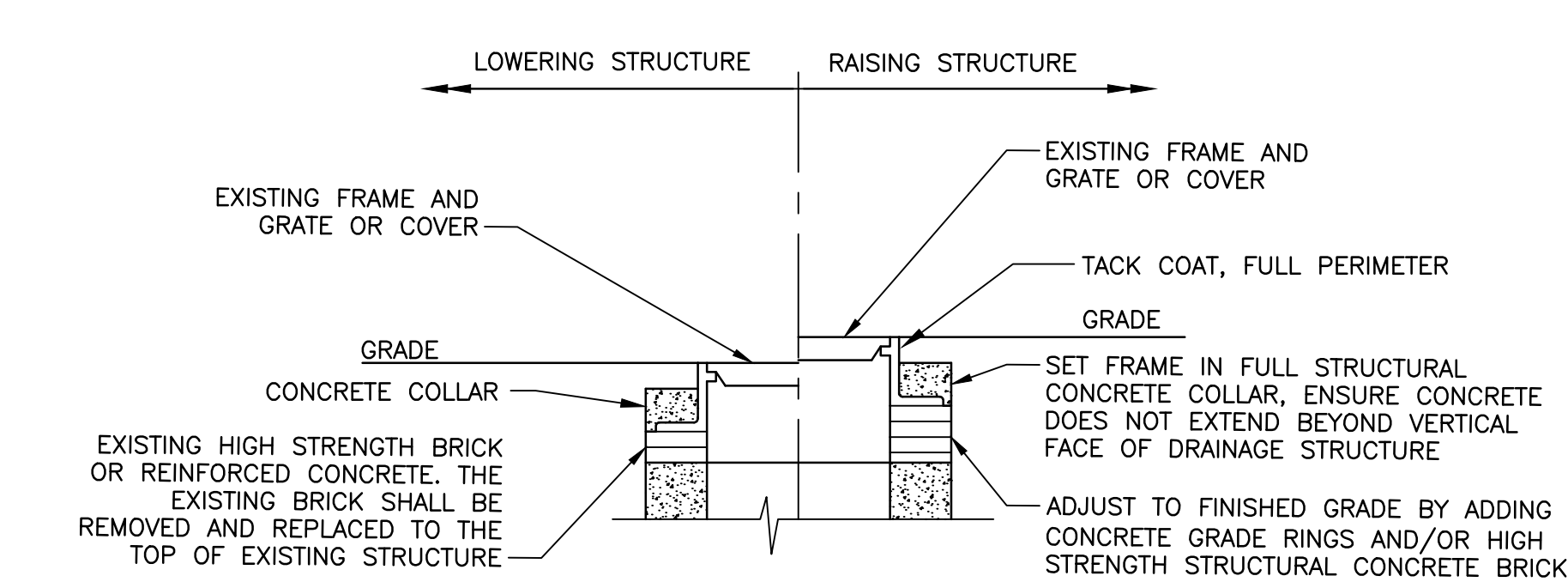
4 TYPICAL TURF ISLAND SECTION
NOT TO SCALE



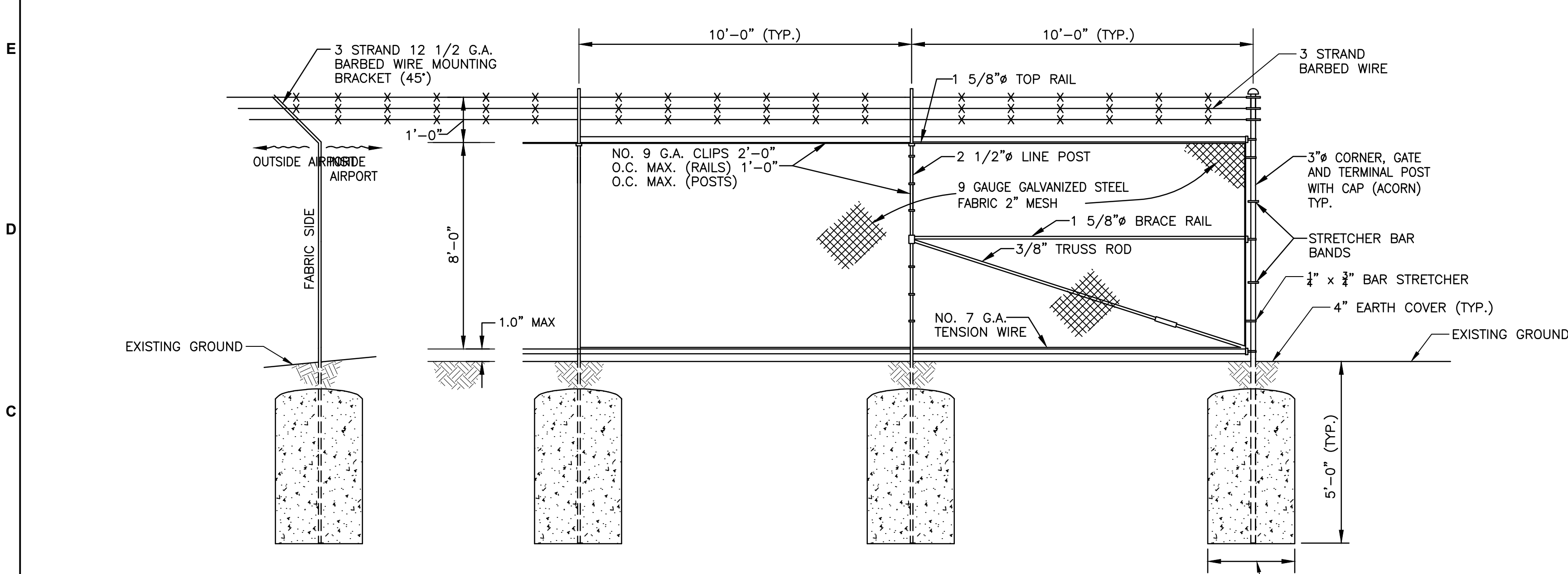
5 TYPICAL PAVEMENT REVEL DETAIL
NOT TO SCALE



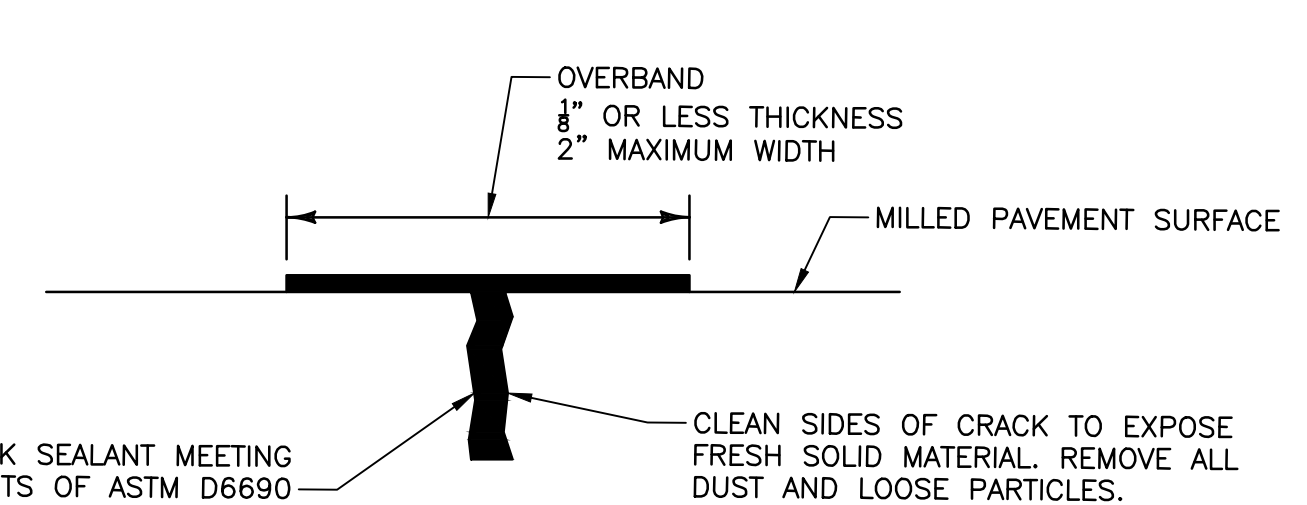
6 BITUMINOUS JOINT SAW AND SEAL DETAIL
NOT TO SCALE



7 ADJUST EXISTING STRUCTURE DETAIL
NOT TO SCALE



8 8' HIGH CHAIN-LINK FENCE DETAIL
NOT TO SCALE



- NOTES:
- ALL CRACKS WILL BE BLOWN CLEAN BY HIGH PRESSURE AIR. ALL LOOSE MATERIAL AND OTHER MATERIAL WILL BE REMOVED FROM THE CRACKS AND FROM THE PAVEMENT SURFACES. THE CRACKS WILL BE STERILIZED BY USE OF PROPANE TORCH TO ELIMINATE ALL VEGETATION, MOISTURE AND DIRT.
 - NO CRACK SEALING MATERIAL WILL BE APPLIED IN WET CRACKS OR WHERE FROST, SNOW OR ICE IS PRESENT NOR WHEN THE AMBIENT TEMPERATURE IS BELOW 40°F.
 - JOINT SEALING MATERIAL WILL BE HEATED AND APPLIED AT A TEMPERATURE SPECIFIED BY THE MANUFACTURER AND APPROVED BY THE ENGINEER. MINIMUM APPLICATION TEMPERATURE WILL BE 320°F.
 - SEALER WILL BE DELIVERED TO THE PAVEMENT SURFACE THROUGH A PRESSURE HOSE LINE AND APPLICATOR SHOE, SIZED AS SPECIFIED.
 - ALL CRACKS WILL BE SEALED AS SPECIFIED HEREIN, AND THE SEALER WILL BE BONDED TO THE PAVEMENT. A WELL BONDED SEALANT CANNOT BE READILY PULLED FROM THE PAVEMENT BY HAND. FOR TYPE 1, CRACKS WILL BE COMPLETELY FILLED FLUSH WITH THE SURFACE OF THE PAVEMENT. MORE THAN ONE APPLICATION OF CRACK SEALER MAY BE NECESSARY TO FILL CRACKS TO REQUIRE LEVEL. ANY OVERBAND MATERIAL WILL BE KEPT TO A MINIMUM THICKNESS OF 1/8" INCH OR LESS AND A MAXIMUM 2 INCH WIDTH. OVERBANDS WILL BE ROLLED WITH A STEEL-WHEEL 1 OR 2 TON ROLLER TO SET THE MATERIAL WHILE HOT. EXCESS OR SPILLED SEALER WILL BE REMOVED FROM THE PAVEMENT BY APPROVED METHODS AND DISCARDED.
 - DO NOT ALLOW TRAFFIC UNTIL THE SEALANTS HAVE CURED.

9 CRACK REPAIR ON MILLED SURFACE DETAIL
NOT TO SCALE

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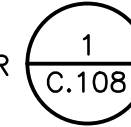
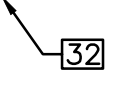

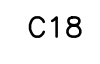
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Client/Project	MERIDEN MARKHAM MUNICIPAL AIRPORT
	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
CAM Dwn.	PVE Chkd.
CAM Dsgn.	2019.05.09 YY.MM.DD

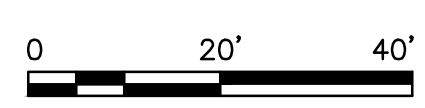
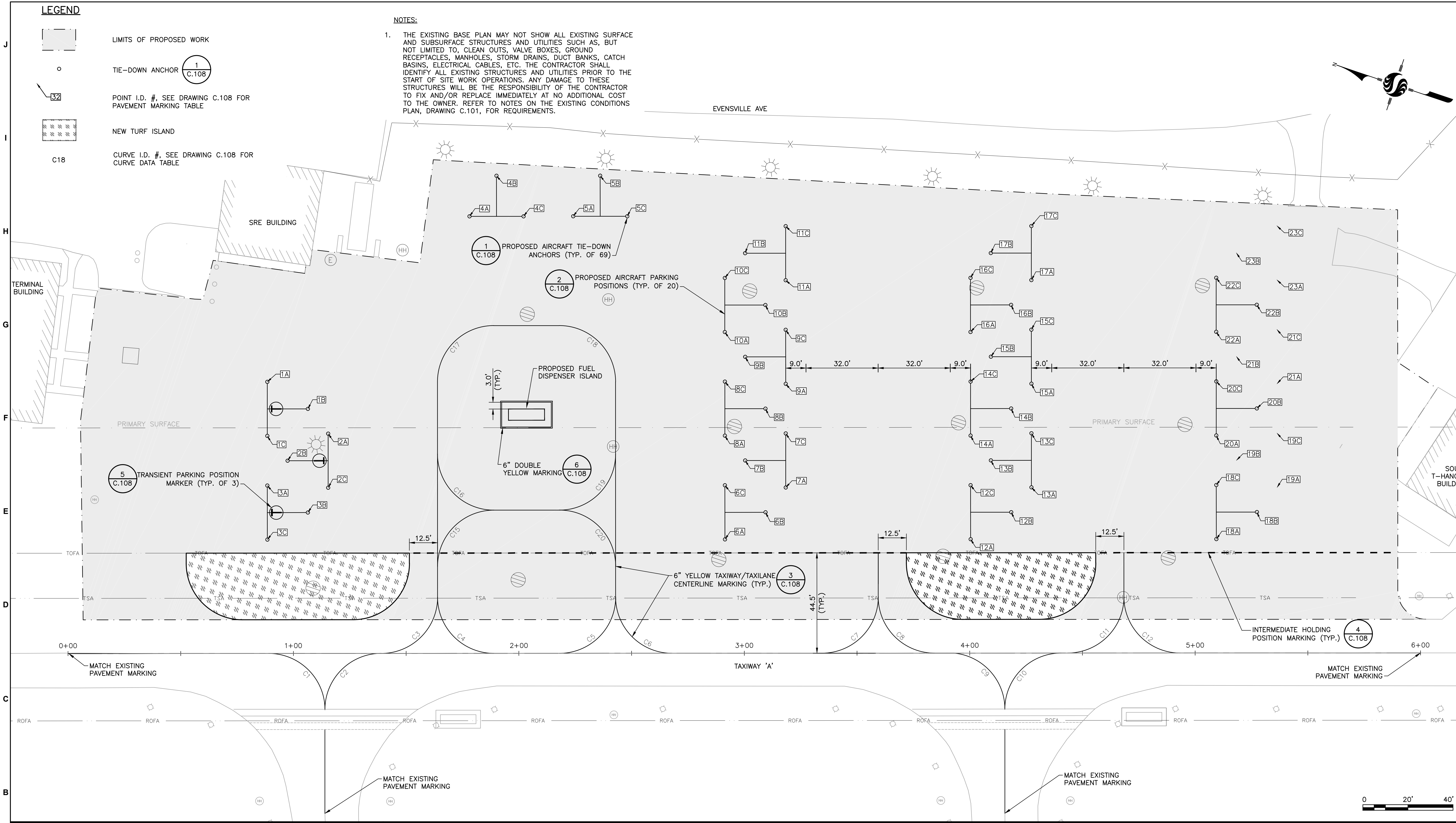
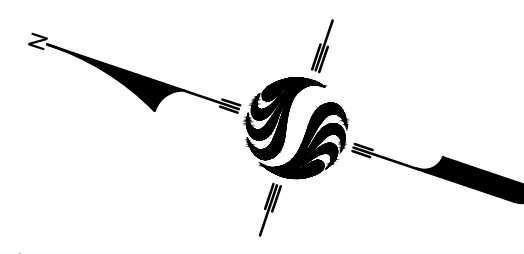
Title		TYPICAL SECTIONS AND DETAILS	
Project No.	Scale	AS SHOWN	
195211190			
Drawing No.	Sheet	Revision	
C.106	10 of 23	0	

LEGEND

-  LIMITS OF PROPOSED WORK
-  TIE-DOWN ANCHOR (1 C.108)
-  POINT I.D. #, SEE DRAWING C.108 FOR PAVEMENT MARKING TABLE
-  NEW TURF ISLAND
-  C18 CURVE I.D. #, SEE DRAWING C.108 FOR CURVE DATA TABLE

NOTES:

1. THE EXISTING BASE PLAN MAY NOT SHOW ALL EXISTING SURFACE AND SUBSURFACE STRUCTURES AND UTILITIES SUCH AS, BUT NOT LIMITED TO, CLEAN OUTS, VALVE BOXES, GROUND RECEPTACLES, MANHOLES, STORM DRAINS, DUCT BANKS, CATCH BASINS, ELECTRICAL CABLES, ETC. THE CONTRACTOR SHALL IDENTIFY ALL EXISTING STRUCTURES AND UTILITIES PRIOR TO THE START OF SITE WORK OPERATIONS. ANY DAMAGE TO THESE STRUCTURES WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIX AND/OR REPLACE IMMEDIATELY AT NO ADDITIONAL COST TO THE OWNER. REFER TO NOTES ON THE EXISTING CONDITIONS PLAN, DRAWING C.101, FOR REQUIREMENTS.



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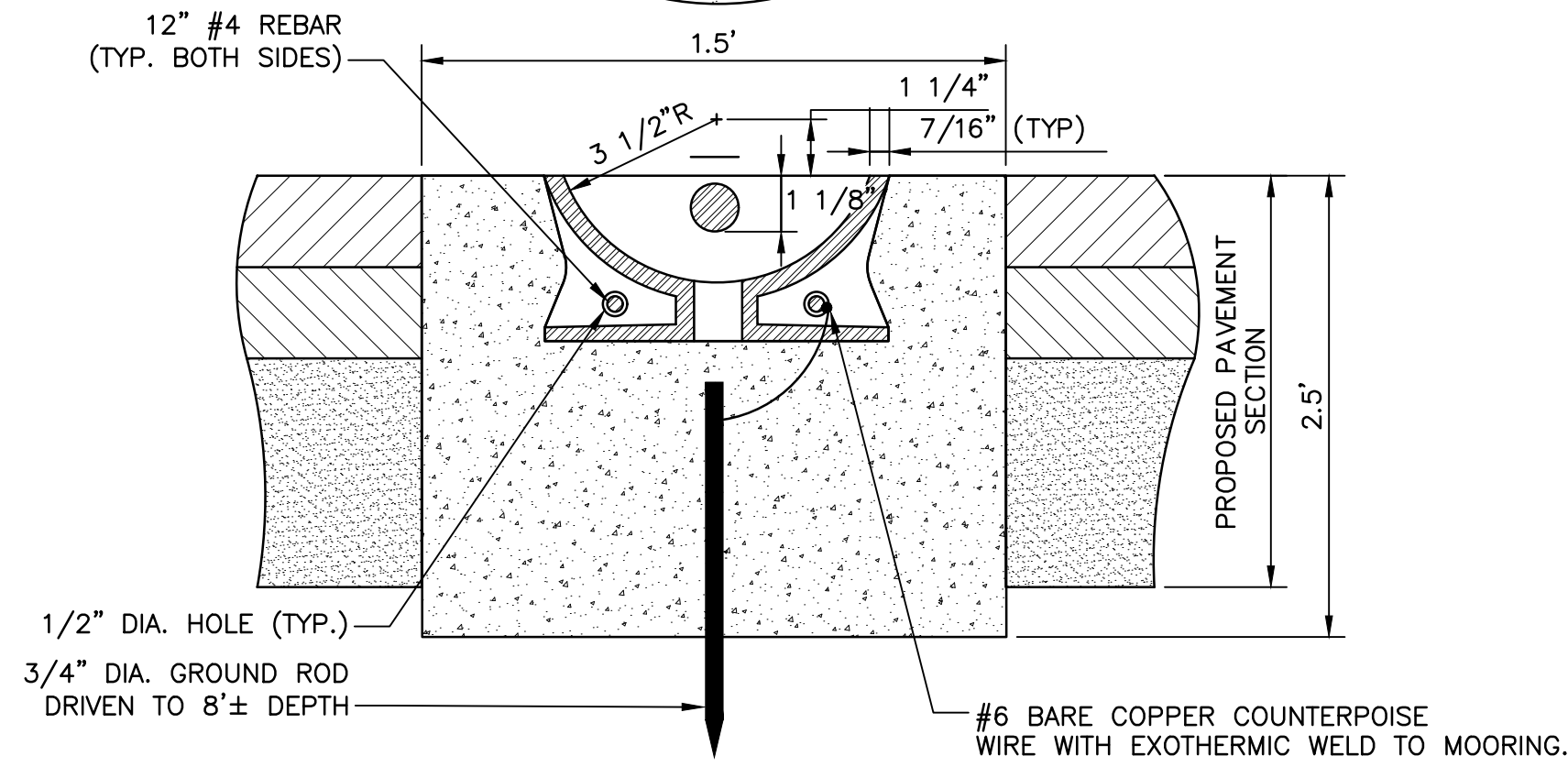
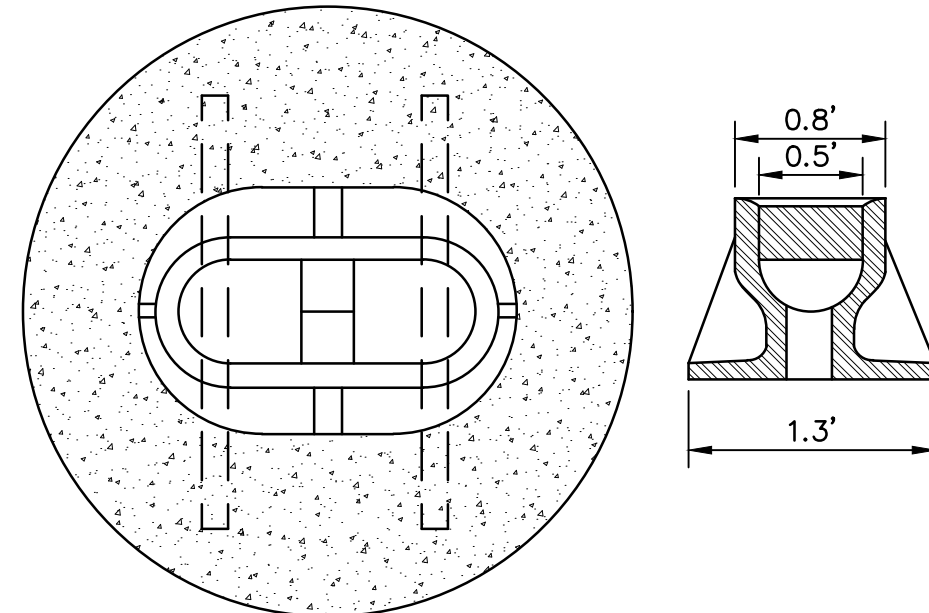
Client/Project	MERIDEN MARKHAM MUNICIPAL AIRPORT
	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
CAM Dwn.	PVE Chkd.
CAM Dsgn.	2019.05.09 YY.MM.DD

Title		
PAVEMENT MARKING PLAN		
Project No.	Scale	
195211190	1" = 20'	
Drawing No.	Sheet	Revision
C.107	11 of 23	0

Tie-Down Anchor Table			
Anchor #	Anchor Designation #	Northing	Easting
1	1A	747661.85	978000.16
2	1B	747641.00	977994.40
3	1C	747654.33	977977.37
4	2A	747629.00	977986.78
5	2B	747642.34	977969.79
6	2C	747621.48	977963.94
7	3A	747647.44	977956.48
8	3B	747626.58	977950.72
9	3C	747639.92	977933.69
10	4A	747599.63	978097.99
11	4B	747593.88	978118.84
12	4C	747576.84	978105.51
13	5A	747555.95	978112.41
14	5B	747550.19	978133.26
15	5C	747533.16	978119.93
16	6A	747447.14	977997.29
17	6B	747433.80	978014.32
18	6C	747454.66	978020.08
19	7A	747428.70	978027.59
20	7B	747449.56	978033.34
21	7C	747436.22	978050.38
22	8A	747461.55	978040.97
23	8B	747448.22	978058.01
24	8C	747469.07	978063.76
25	9A	747443.12	978071.27
26	9B	747463.97	978077.03
27	9C	747450.63	978094.06
28	10A	747475.96	978084.65
29	10B	747462.63	978101.69
30	10C	747483.48	978107.45
31	11A	747457.53	978114.96
32	11B	747478.38	978120.71
33	11C	747465.05	978137.75
34	12A	747343.63	978031.44
35	12B	747330.29	978048.47

Tie-Down Anchor Table			
Anchor #	Anchor Designation #	Northing	Easting
36	12C	747351.15	978054.23
37	13A	747325.19	978061.74
38	13B	747346.04	978067.50
39	13C	747332.71	978084.53
40	14A	747358.04	978075.12
41	14B	747344.70	978092.16
42	14C	747365.56	978097.91
43	15A	747339.60	978105.42
44	15B	747360.46	978111.18
45	15C	747347.12	978128.21
46	16A	747372.45	978118.81
47	16B	747359.12	978135.84
48	16C	747379.97	978141.60
49	17A	747354.02	978149.11
50	17B	747374.87	978154.86
51	17C	747361.54	978171.90
52	18A	747240.11	978065.59
53	18B	747226.78	978082.62
54	18C	747247.63	978088.38
55	19A	747221.68	978095.89
56	19B	747242.53	978101.65
57	19C	747229.20	978118.68
58	20A	747254.53	978109.27
59	20B	747241.19	978126.31
60	20C	747262.05	978132.06
61	21A	747236.09	978139.57
62	21B	747256.95	978145.33
63	21C	747243.61	978162.37
64	22A	747268.94	978152.96
65	22B	747255.60	978169.99
66	22C	747276.46	978175.75
67	23A	747250.50	978183.26
68	23B	747271.36	978189.01
69	23C	747258.02	978206.05

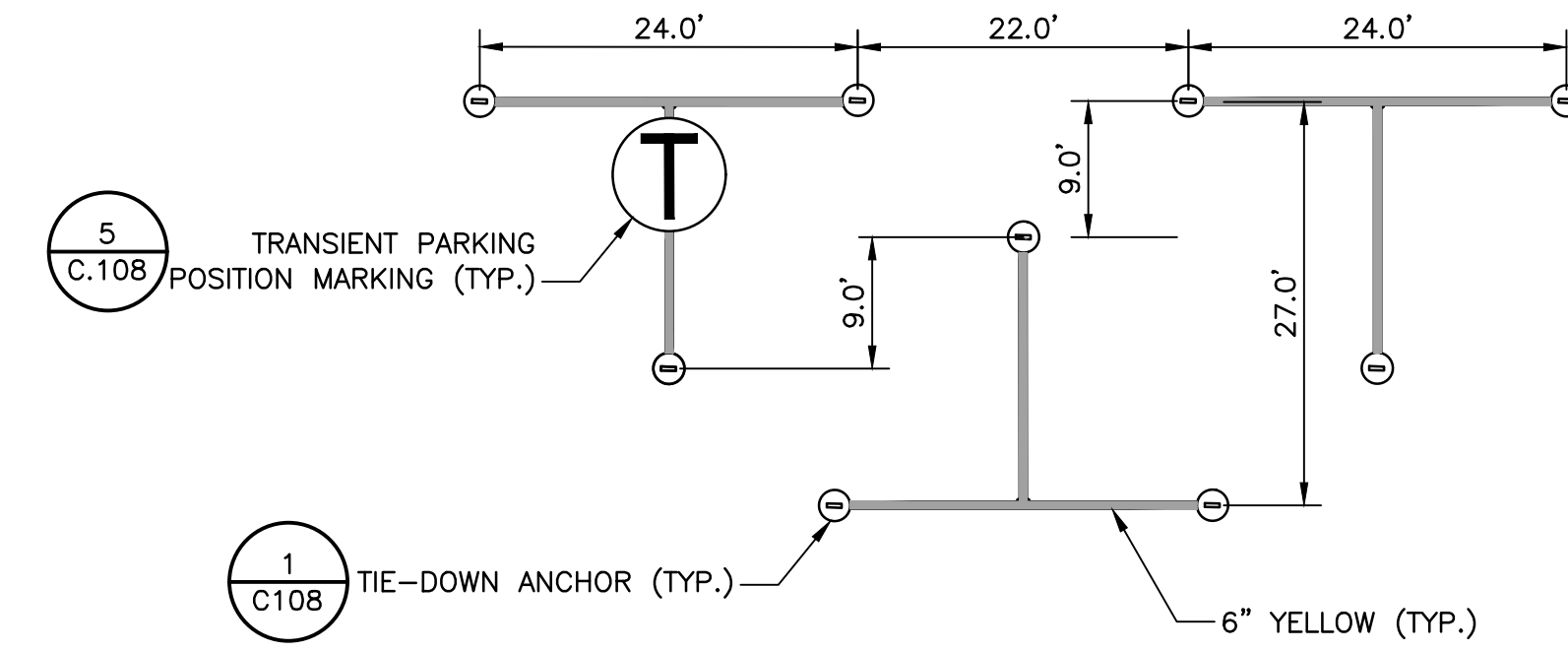
Curve Table							
Curve #	Length	Radius	Delta	PC Northing	PC Easting	PT Northing	PT Easting
C1	39.27	25.0	90.0	747592.04	977869.95	747623.61	977885.86
C2	39.27	25.0	90.0	747576.13	977901.52	747592.04	977869.95
C3	39.27	25.0	90.0	747576.13	977901.52	747560.22	977933.09
C4	39.27	25.0	90.0	747560.22	977933.09	747528.65	977917.19
C5	39.27	25.0	90.0	747501.11	977926.27	747485.20	977957.85
C6	39.27	25.0	90.0	747485.20	977957.85	747453.62	977941.94
C7	39.88	25.0	91.4	747391.05	977962.58	747374.56	977994.34
C8	39.27	25.0	90.0	747374.56	977994.35	747342.99	977978.44
C9	36.78	25.0	84.3	747305.61	977964.47	747334.79	977981.14
C10	39.27	25.0	90.0	747289.67	977996.03	747305.58	977964.46
C11	39.27	25.0	90.0	747286.96	977996.92	747271.05	978028.50
C12	39.27	25.0	90.0	747271.05	978028.50	747239.48	978012.59
C13	40.87	25.0	93.7	747184.96	978030.57	747167.52	978062.60
C14	39.27	25.0	90.0	747167.54	978062.65	747135.96	978046.74
C15	39.27	25.0	90.0	747548.56	977977.54	747564.46	977945.96
C16	39.27	25.0	90.0	747580.13	977993.45	747548.55	977977.54
C17	39.27	25.0	90.0	747574.22	978055.30	747590.12	978023.73
C18	39.27	25.0	90.0	747515.10	978048.49	747546.68	978064.39
C19	39.27	25.0	90.0	747521.02	977986.63	747505.11	978018.20
C20	39.27	25.0	90.0	747489.44	977970.72	747521.02	977986.63



NOTES:

- TIE-DOWN INSTALLATIONS SHALL BE PERFORMED BY CORE DRILLING THE 1.5" DIA HOLE. NO AUGERS SHALL BE USED.
- TIE-DOWN ANCHOR SUPPLIED BY SYRACUSE CASTING CATALOG NO. R-3490 OR APPROVED EQUAL.

1 TIE-DOWN ANCHOR DETAIL
NOT TO SCALE



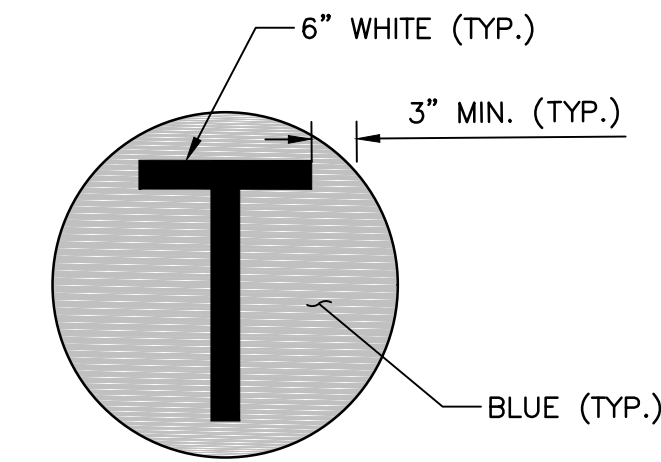
2 TIE-DOWN ANCHOR LAYOUT DETAIL
NOT TO SCALE



3 TAXIWAY/TAXILANE CENTERLINE MARKING
NOT TO SCALE



4 INTERMEDIATE HOLDING POSITION MARKING DETAIL
NOT TO SCALE



NOTES:

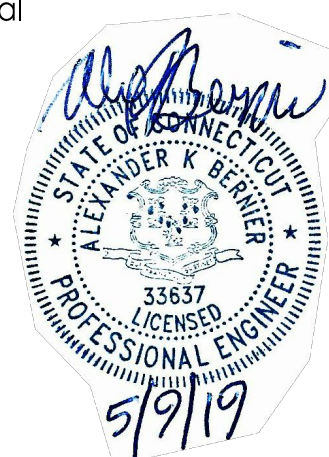
- TRANSIENT PARKING POSITION MARKING SHALL BE CIRCULAR WITH A 3.0' DIAMETER.
- FIELD MEASURE EXISTING TRANSIENT PARKING POSITION MARKERS TO CONFIRM DIMENSIONS.

5 TRANSIENT PARKING POSITION MARKING
NOT TO SCALE



5 6" DOUBLE YELLOW MARKING
NOT TO SCALE

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Client/Project

MERIDEN MARKHAM MUNICIPAL AIRPORT

MID-BASE APRON REHABILITATION

MERIDEN, CONNECTICUT

File Name: 195211190

CAM Dwn. PVE Chkd. CAM Dsgn. 2019.05.09 YY.MM.DD

Title

PAVEMENT MARKING TABLES AND DETAILS

Project No.
195211190

Scale
AS SHOWN

Drawing No.

Sheet

Revision

C.108

12 of 23

0

Revision

By Appd. MM.DD.YY

Issued

By Appd. YY.MM.DD

FUEL SYSTEM GENERAL NOTES

- PROVIDE ALL MATERIALS AND EQUIPMENT AND PERFORM ALL LABOR REQUIRED TO INSTALL COMPLETE AND OPERABLE FUEL SYSTEMS AS INDICATED ON THE CONTRACT DOCUMENTS AND AS REQUIRED BY CODE.
- INSTALL AND TEST ALL FUEL SYSTEM EQUIPMENT AND APPURTENANCES IN ACCORDANCE WITH MANUFACTURER'S WRITTEN RECOMMENDATIONS, CONTRACT DOCUMENTS, AND APPLICABLE CODES AND REGULATIONS.
- THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK AND REFER TO CIVIL ENGINEERING PLANS FOR THE LOCATION OF ALL EXISTING AND PROPOSED UTILITIES. THE CONTRACTOR SHALL PAY FOR AND REPAIR ALL DAMAGES CAUSED BY FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES UNLESS OTHERWISE INDICATED.
- WHERE TWO OR MORE ITEMS OF THE SAME TYPE OF EQUIPMENT ARE REQUIRED, THE PRODUCT OF ONE MANUFACTURER SHALL BE USED.
- THE CONTRACTOR SHALL OBTAIN ALL STATE AND LOCAL LICENSES AND CERTIFICATIONS GOVERNING THE TYPE OF WORK TO BE PERFORMED. PROOF OF NECESSARY LICENSES AND CERTIFICATIONS SHALL BE FURNISHED TO THE OWNER PRIOR TO THE START OF SITE WORK.
- THE CONTRACTOR SHALL OBTAIN AND MAINTAIN INSTALLER CERTIFICATIONS AS REQUIRED BY THE EQUIPMENT MANUFACTURERS OUTLINED IN THIS PROJECT, INCLUDING CERTIFICATIONS FOR INSTALLATION OF THE ENVIRONMENTAL MONITORING SYSTEM AND UNDERGROUND PIPING SYSTEMS. PROOF OF NECESSARY MANUFACTURER CERTIFICATIONS SHALL BE FURNISHED TO THE OWNER PRIOR TO THE START OF SITE WORK.
- THE CONTRACTOR SHALL OBTAIN NECESSARY PERMITS FROM ALL GOVERNING FEDERAL, STATE, AND LOCAL JURISDICTIONS AND MAKE ARRANGEMENTS FOR INSPECTIONS AND APPROVALS FOR THE WORK. WHERE APPLICABLE, THIS INCLUDES ALL PERMITS/APPROVALS FROM THE LOCAL MUNICIPALITY AND AIRPORT AUTHORITIES. ALL INCURRED COSTS ARE TO BE BORNE BY THE CONTRACTOR. UPON COMPLETION OF THE WORK, THE CONTRACTOR SHALL DELIVER TO THE OWNER ALL NECESSARY CERTIFICATES OF APPROVAL FROM ALL GOVERNING JURISDICTIONS AS PART OF THE PROJECT CLOSEOUT DOCUMENTATION. CONTRACTOR SHALL COORDINATE PERMITTING WITH ENGINEER TO OBTAIN ALL REQUIRED PERMITS.
- THE CONSTRUCTION DRAWINGS COVER THE DESIGN AND INSTALLATION ELEMENTS OF THE PROPERTY IMPROVEMENT PROJECT. ANY DEVIATION FROM THE DRAWINGS MUST BE APPROVED BY THE OWNER PRIOR TO THE CONTRACTOR MAKING ANY SUBSTITUTIONS, CHANGES, OR MODIFICATIONS.
- THE OWNER SHALL, AT ALL TIMES, HAVE ACCESS TO THE WORK, AND THE CONTRACTOR SHALL PROVIDE FOR SUCH ACCESS AND FOR INSPECTION. IF CERTAIN WORK IS FOUND TO BE NOT IN ACCORDANCE WITH THE CONTRACTUAL DOCUMENTS, THE DEFICIENT WORK MUST BE IMMEDIATELY REMOVED AND REPLACED TO THE OWNER'S SATISFACTION.
- ALL TESTS REQUIRED SHALL BE DOCUMENTED ON A TEST REPORT SIGNED BY THE CONTRACTOR. ALL TEST REPORTS SHALL BE SUBMITTED TO THE OWNER WITHIN 3 DAYS OF THE TEST AND INCLUDED IN THE CLOSEOUT DOCUMENTS.
- SHOP DRAWINGS FOR ALL FUEL SYSTEM EQUIPMENT OUTLINED ON THE FUEL SYSTEM EQUIPMENT LIST, SHALL BE SUBMITTED TO THE OWNER. REALIZING THE LONG LEAD TIME ASSOCIATED WITH CERTAIN EQUIPMENT, THE CONTRACTOR SHALL MAKE EVERY EFFORT TO SUBMIT THESE SHOP DRAWINGS AS SOON AS POSSIBLE AFTER THE CONTRACT AWARD DATE. IF THE CONTRACTOR DOES NOT SUBMIT SHOP DRAWINGS IN THE ALLOTTED TIME, NO CONSIDERATION WILL BE GIVEN ON THE PROJECT SCHEDULE FOR SUPPLY DELAYS.
- THE CONTRACTOR SHALL PROVIDE CLOSEOUT DOCUMENTATION TO THE OWNER, AT A MINIMUM, WITH ONE (1) HARD COPY AND ONE (1) CD COPY OF THE FOLLOWING CLOSEOUT DOCUMENTATION WITHIN 30 DAYS OF PROJECT COMPLETION. CLOSEOUT DOCUMENTS SHALL BE ORGANIZED INTO HEAVY DUTY VINYL COVERED 3-RING BINDERS OF MANAGEABLE SIZE AS WELL AS ELECTRONIC COPIES (PDF'S) ON A CD. BIND AND INDEX ALL DOCUMENTS. INCLUDE FOLDERS TO ACCOMMODATE FOLDED OR OVERSIZED SHEETS, OR CDS. EACH BINDER SHALL BE LABELED ON THE COVER AND SPINE WITH THE PRINTED TITLE 'CLOSEOUT DOCUMENTS', PROJECT NAME, NUMBER, AND DATE. THE OWNER RESERVES THE RIGHT TO WITHHOLD FINAL PAYMENT TO THE CONTRACTOR UNTIL THE OWNER IS IN RECEIPT OF ALL REQUIRED DOCUMENTATION.
 - MANUFACTURERS INSTALLATION AND SETUP CHECKLISTS FOR ANY SPECIFIED ELECTRONIC OVERFILL PREVENTION DEVICE, INCLUDING CERTIFICATION OF OPERATION IF REQUIRED BY AUTHORITIES HAVING JURISDICTION.
 - COPIES OF ALL PERMITS AND CERTIFICATES OF INSPECTION AND/OR APPROVAL.
 - A MINIMUM OF TWELVE (24) PHOTOGRAPHS OF KEY PHASES OF THE PROJECT SUCH AS; DEMOLITION, SYSTEM COMPONENT AND EQUIPMENT INSTALLATION (I.E., EQUIPMENT PLACEMENT, PIPING RUNS AND CONNECTIONS, ETC.).
 - AS-BUILT CONSTRUCTION DRAWINGS. ALL SHEETS SHALL BE SUBMITTED, EVEN IF THERE ARE NO CHANGES, EACH SIGNED AND DATED BY THE CONTRACTOR. IF ANY SHEETS DO NOT REQUIRE EDITING, THE CONTRACTOR SHALL INDICATE "NO CHANGES THIS SHEET" THEN SIGN AND DATE THE NOTATION.
 - PIPING TEST RESULTS AND TEST RESULTS FOR ANY SECONDARY CONTAINMENT STRUCTURE OR ANNULUS AND CONTAINMENT SUMPS IF INDICATED.
 - WARRANTIES FOR ALL EQUIPMENT AND APPARATUS. IN GENERAL, ANY PRODUCT/MANUFACTURER DOCUMENTATION THAT WAS PROVIDED WITH THE EQUIPMENT SHALL BE PROVIDED AS PART OF THE CLOSEOUT DOCUMENTS.
 - ALL INSTRUCTION BULLETINS, PREVENTIVE MAINTENANCE SCHEDULES, OPERATIONAL INSTRUCTIONS, AND PARTS LISTS PROVIDED WITH THE, DISPENSERS, AND ENVIRONMENTAL MONITORING SYSTEM (ONLY 1 COPY OF OWNER OPERATING MANUALS REQUIRED).
 - WASTE DISPOSAL DOCUMENTATION (IF ANY).
 - OTHER ENVIRONMENTAL INFORMATION (IF ANY).
 - THE CONTRACTOR SHALL PROVIDE A GENERAL RELEASE OF LIENS, ALONG WITH A RELEASE OF LIENS FOR ALL SUBCONTRACTORS.
 - COPIES OF RECEIPTS FOR ANY KEYS, LOCKS, OR OTHER EQUIPMENT TURNED OVER TO THE OWNER.
 - COMPLETE CLOSEOUT DOCUMENTATION AS REQUIRED BY OWNER.
- CONTRACTOR SHALL GUARANTEE ALL MATERIALS AND WORKMANSHIP FREE FROM DEFECTS FOR A PERIOD OF NOT LESS THAN ONE YEAR FROM DATE OF ACCEPTANCE, UNLESS INDICATED OR SPECIFIED OTHERWISE.
- IN CASE OF DISCREPANCY BETWEEN THESE CONTRACT DOCUMENTS, CALCULATED DIMENSIONS, UNLESS OBVIOUSLY INCORRECT, WILL GOVERN OVER SCALED DIMENSIONS.
- THE CONTRACTOR SHALL NOT TAKE ADVANTAGE OF ANY APPARENT ERROR OR OMISSION IN THE CONTRACT. IF AN ERROR OR OMISSION IS DISCOVERED, THE ENGINEER SHALL BE PROMPTLY NOTIFIED SO CORRECTIONS AND INTERPRETATIONS NECESSARY TO FULFILL THE INTENT OF THE CONTRACT CAN BE MADE.
- THESE DRAWINGS SHALL BE USED IN CONJUNCTION WITH THE FUEL AND RELATED SYSTEMS SPECIFICATION SECTIONS.
- ALL EQUIPMENT SHALL BE INSTALLED PER MANUFACTURER'S WRITTEN RECOMMENDATIONS.
- PIPING MUST BE PNEUMATICALLY TESTED TO 100% OF THE MAXIMUM OPERATING PRESSURE OR A MINIMUM OF PRESSURE OF 5 POUNDS PER SQUARE INCH AND IN ACCORDANCE WITH MANUFACTURERS REQUIREMENTS.
- PIPING SYSTEMS SHALL BE COMPATIBLE WITH THE SUBSTANCE STORED AND BE PROTECTED FROM FAILURE DUE TO INTERNAL AND EXTERNAL WEAR, VIBRATION, SHOCK AND CORROSION.
- PIPING SYSTEMS SHALL BE FREE OF LEAKAGE, STRUCTURALLY SOUND, PROPERLY SUPPORTED UNDER ALL OPERATING CONDITIONS AND BE PROTECTED FROM FIRE, HEAT, VACUUM AND PRESSURE THAT WOULD CAUSE THE SYSTEM TO FAIL.
- PIPING SYSTEMS SHALL BE DESIGNED, INSTALLED AND MAINTAINED TO PREVENT DAMAGE FROM EXPANSION, JARRING, VIBRATION, CONTRACTION AND FROST.
- PIPING SYSTEMS SHALL BE PROTECTED FROM COLLISION DAMAGE OR CRUSHING LOADS BY VEHICLES AND EQUIPMENT.
- JOINT COMPOUNDS AND GASKETS SHALL BE COMPATIBLE WITH THE SUBSTANCE STORED.
- PIPING WITH PUMP CONNECTIONS SHALL BE PROVIDED WITH SHUT-OFF VALVES LOCATED ADJACENT TO THE CONNECTIONS.
- FLEXIBLE CONNECTORS, ELBOWS, LOOPS, EXPANSION CHAMBERS OR SIMILAR MEASURES SHALL BE INSTALLED TO ALLOW FOR MOVEMENT AND PREVENT DAMAGE FROM WATER HAMMER WHERE NECESSARY.
- PRIOR TO OPERATIONS, SYSTEM SHALL BE TESTED IN PRESENCE OF THE INSPECTOR IF REQUIRED. TO SET APPOINTMENT FOR ACCEPTANCE TEST PLEASE CONTACT THE INSPECTOR FOR THE PROPOSED LOCATION.
- SYSTEM SHALL BE INSTALLED IN COMPLIANCE WITH NFPA 30, NFPA 407, AND THE INTERNATIONAL FIRE CODE.

Fuel System Environmental Monitoring Equipment

Manufacturer:	Model:	Description:
FE Petro	EVO	Environmental monitoring console
FE Petro	FMP-LL3-###	Automatic Tank Gauge - order length to suit AST
FE Petro	FMP-ULS	Universal sump leak sensor
FE Petro	FMP-UHS	Interstitial Leak Sensor
FE Petro	TS-RA1	High level alarm with light and horn
FE Petro	TS-RK	Acknowledgement switch

Dispenser & Related Equipment

Manufacturer:	Model:	Description:
Bennett	3812SNR-21-P	1-Product ,(2)-Hose Avgas 100LL Remote Dispenser, no internal filter, 100:1 pulse output and stainless steel panels
OPW	295SA-0135	1" Fueling Nozzle (AVGAS) with built-in-swivel, and 100 mesh strainer
OPW	45-5075	1" 45 degree hose swivel
Peco Facet	VF-21SB-PGS	Fuel Filter with differential pressure gauge and sight glass
Goodyear	Wingcraft	1" x 75' long aviation fueling hose
OPW	FC10-MM##	1" Stainless Flexible connector
OPW	FC20-MM##	2" Stainless Flexible Connector
OPW	10P-0152	Dispenser emergency shear valve
Hannay	V-EPJ 6024-23-24 RT	Electric hose reel for 1" hose with roller option
OPW	Order to suit	16' x 6' Stainless steel island form with 10" reveal
Bravo	B1000 series	FRP under dispenser containment sump - order to fit dispenser
Bravo	BK-1011	Stabilizer Bar and bracket assembly for dispenser and sump
Bravo	F-series	FRP sump entry fittings for brugg pipe and conduits
Bravo	B-500	FRP transition sump with rack system frame option
Gammon	GTP-9394-##	Manual grounding reel - order ## for correct side on dispenser island
Brugg	HL 60/83	2" Double-walled stainless steel corrugated fuel safety pipe, include end fittings.

Miscellaneous Equipment

Manufacturer:	Model:	Description:
Fuel Management Pedestal	Qtpod M4000	Fuel Management Pedestal
Power Integrity	IA-ESOC	Emergency stop button with cover

EQUIPMENT LIST NOTES:

THE EQUIPMENT SHOWN IN THIS TABLE INDICATES MAJOR EQUIPMENT ONLY. THE CONTRACTOR SHALL PROVIDE ALL OTHER EQUIPMENT NECESSARY FOR A COMPLETE FUEL SYSTEM. CONTRACTOR TO VERIFY THAT ALL EQUIPMENT WILL FUNCTION PROPERLY WITHIN THE SYSTEM AS DESIGNED. MODEL NUMBERS ARE SUBJECT TO CHANGE; THE CONTRACTOR SHALL ALERT THE ENGINEER IF ANY MODEL NUMBERS LISTED NO LONGER EXIST FROM THE MANUFACTURER OR IF THE ITEM IS NO LONGER AVAILABLE.

A

Revision	By	Appd.	MM.DD.YY	Issued	By	Appd.	YY.MM.DD

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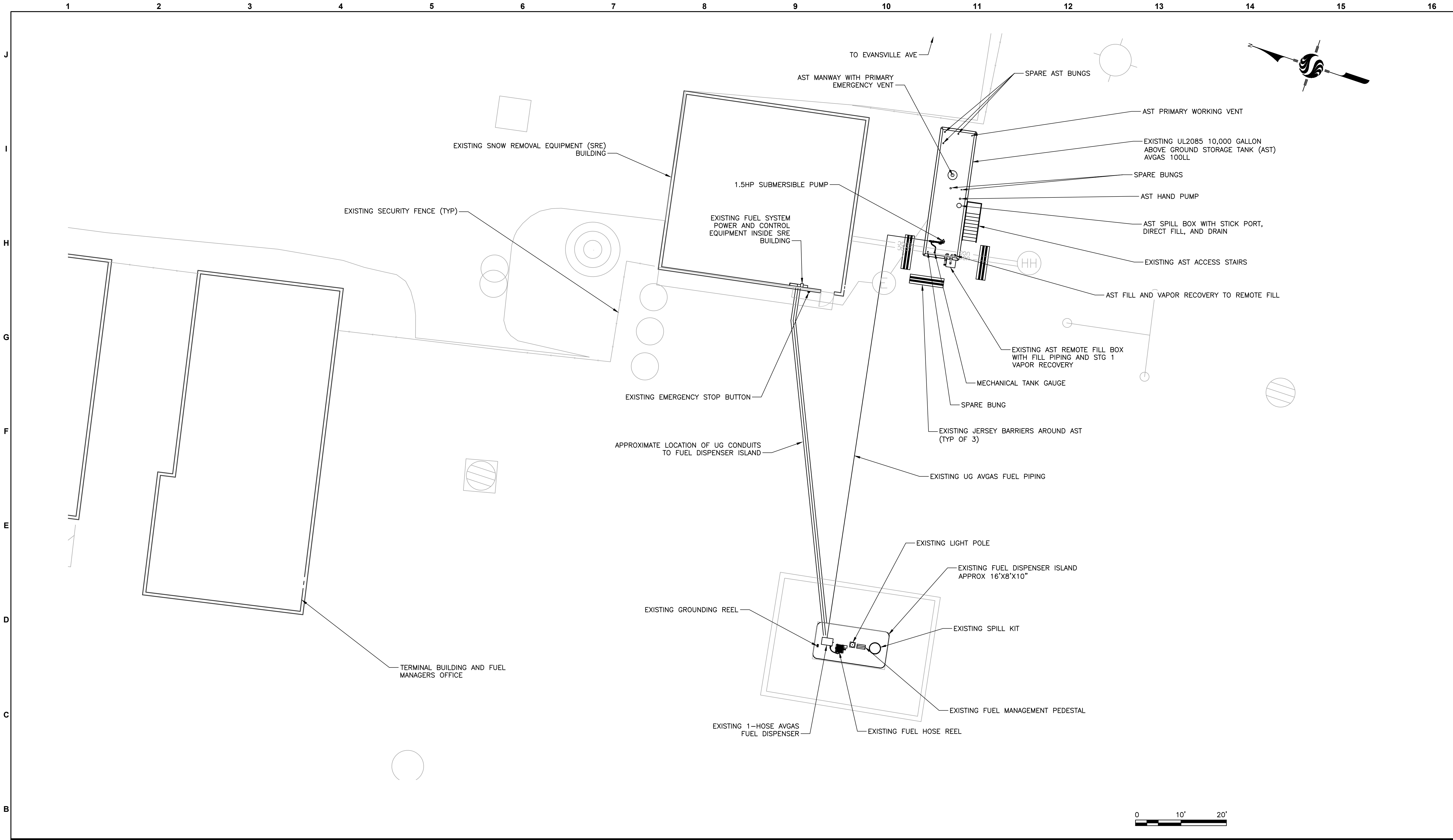
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Client/Project	MERIDEN MARKHAM MUNICIPAL AIRPORT
	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
NRD Dwn.	NIM Chkd.
NRD Dsgn.	2019.05.09 YY.MM.DD

Title		
FUEL SYSTEM GENERAL NOTES AND MAJOR EQUIPMENT LIST		
Project No.	Scale	
195211190	NONE	
Drawing No.	Sheet	Revision
FS.0	13 of 23	0



Revision	By	Appd.	MM.DD.YY

Issued	By	Appd.	YY.MM.DD

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MMK 5/8/19

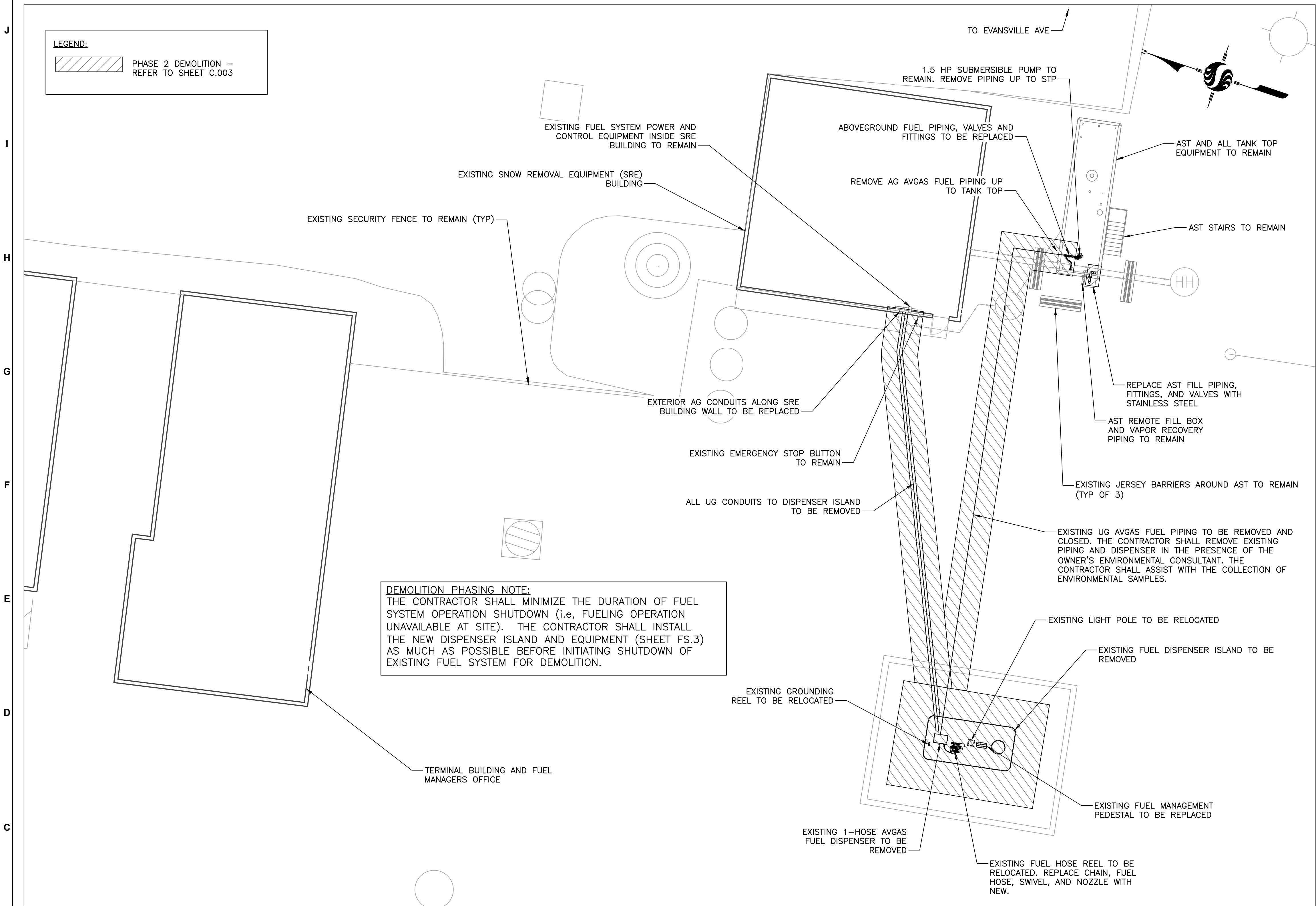
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	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
Dwn.	NRD
Chkd.	NTM
Dsgn.	NRD
	2019.05.09
	YY.MM.DD

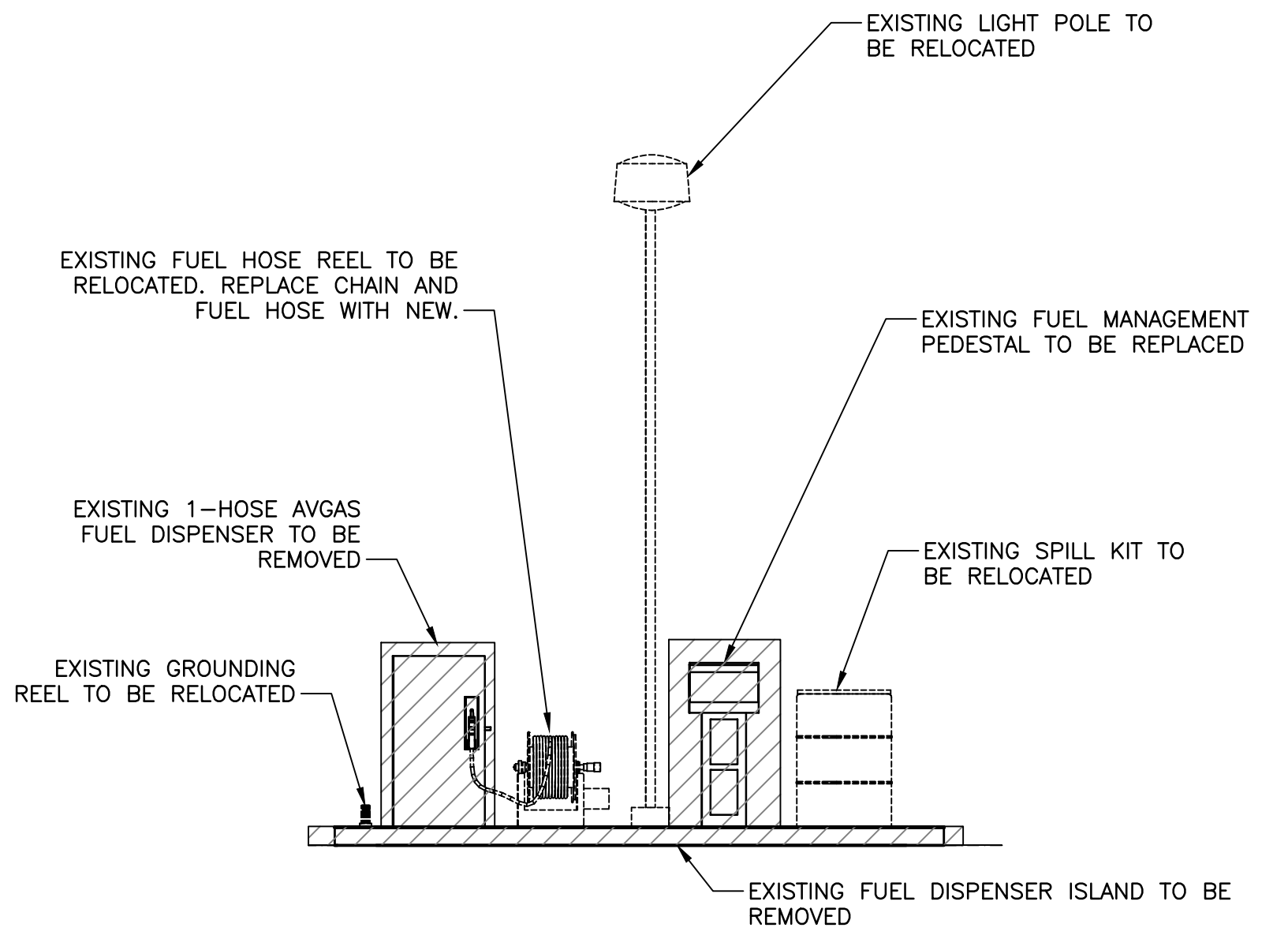
Title		
FUEL SYSTEM EXISTING CONDITIONS PLAN		
Project No.	Scale	
195211190	1" = 10'	
Drawing No.	Sheet	Revision
FS.1	14 of 23	0



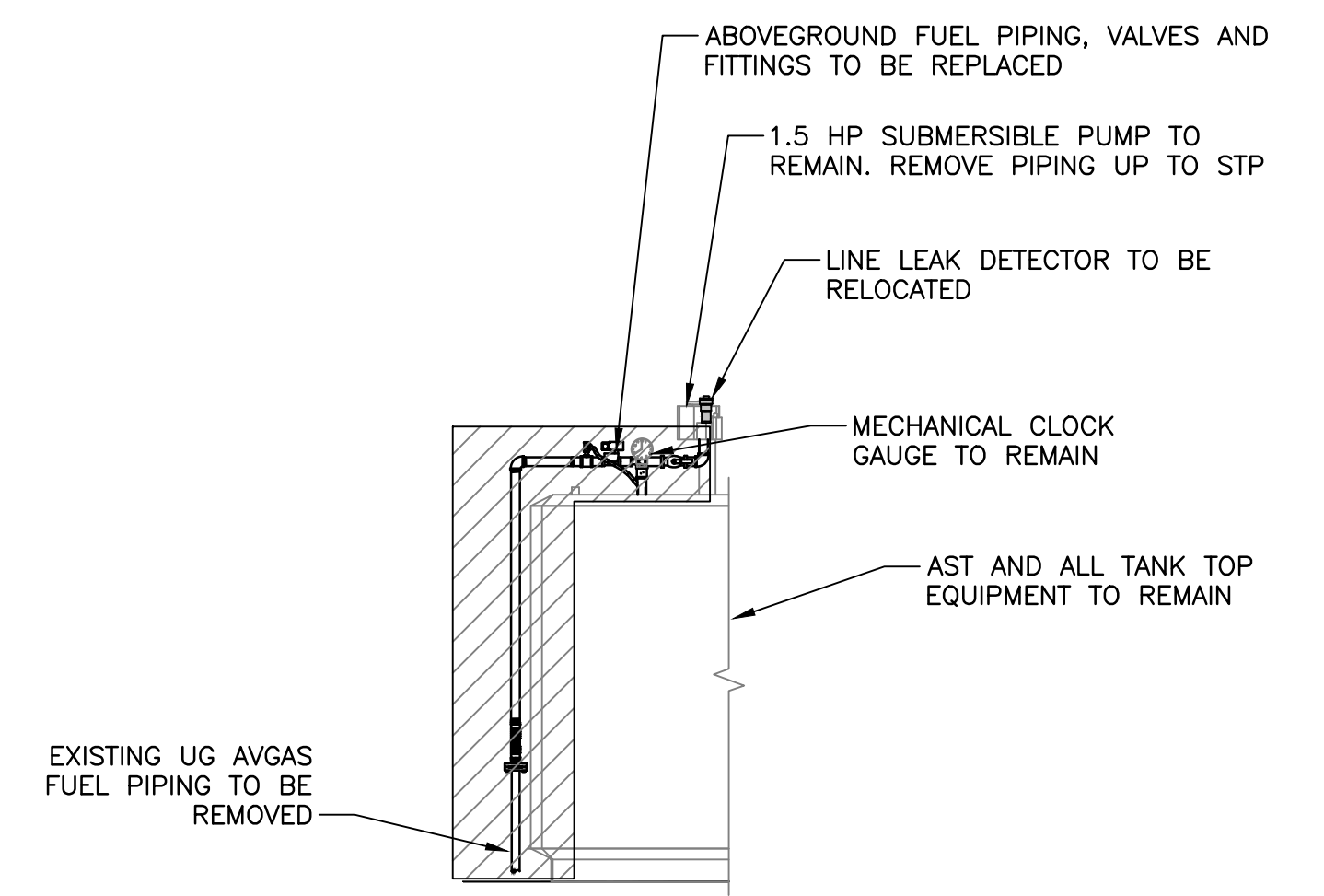
LEGEND:
 PHASE 2 DEMOLITION - REFER TO SHEET C.003

DEMOLITION PHASING NOTE:
 THE CONTRACTOR SHALL MINIMIZE THE DURATION OF FUEL SYSTEM OPERATION SHUTDOWN (i.e., FUELING OPERATION UNAVAILABLE AT SITE). THE CONTRACTOR SHALL INSTALL THE NEW DISPENSER ISLAND AND EQUIPMENT (SHEET FS.3) AS MUCH AS POSSIBLE BEFORE INITIATING SHUTDOWN OF EXISTING FUEL SYSTEM FOR DEMOLITION.

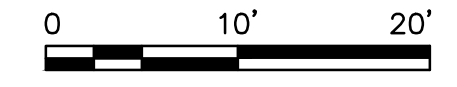
1 FUEL SYSTEM DEMOLITION PLAN
 SCALE: 1" = 10'
 DEMOLITION



2 FUEL ISLAND DEMOLITION ELEVATION
 SCALE: 1" = 4'
 DEMOLITION



3 AST PIPING DEMOLITION ELEVATION
 SCALE: 1" = 4'
 DEMOLITION



Revision	By	Appd.	MM.DD.YY	Issued	By	Appd.	YY.MM.DD

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Client/Project	MERIDEN MARKHAM MUNICIPAL AIRPORT
	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
NRD Dwn.	NRD Chkd.
NRD Dsgn.	2019.05.09 YY.MM.DD

Title	
FUEL SYSTEM DEMOLITION PLAN (PHASE 2)	
Project No.	Scale
195211190	1" = 10'
Drawing No.	Sheet
FS.2	15 of 23
Revision	0

LEGEND:
 _____ PIPING
 - - - - - CONDUIT

1
FS.5
HIGH LEVEL ALARM AND ACKNOWLEDGMENT SWITCH MOUNTED TO BUILDING EXTERIOR 48" AFF

FRP TRANSITION SUMP WITH CONCRETE FOR AG-UG PIPING TRANSITION

INTERSTITIAL MONITOR TO BE INSTALLED IN AST INTERSTITIAL BUNG

AUTOMATIC TANK GAUGE TO BE INSTALLED IN AST BUNG

EXISTING UL2085 10,000 GALLON ABOVE GROUND STORAGE TANK (AST) AVGAS 100LL

EXISTING AST STAIRS

EXISTING JERSEY BARRIERS AROUND AST (TYP OF 3)

4" BOLLARD (TYP OF 2) 2
FS.3

EMERGENCY STOP BUTTON ON POST 48" AFF AND WITHIN 100' OF FUEL DISPENSER 2
FS.5

2" AG SCH40 SS PIPING, VALVES AND FITTINGS. CONNECT INTO EXISTING STP.

SLOPE UG FUEL PIPING 1/8" PER FOOT TO TRANSITION SUMP

2" UG DW FLEXIBLE STAINLESS STEEL SAFETY PIPING AVGAS 100LL 6
FS.4

POWER, COMMUNICATIONS, INTRINSICALLY SAFE, AND (2) 1" SPARE CONDUITS TO FUEL DISPENSER ISLAND REFER TO FS.6

GROUNDING REEL

2-HOSE AVGAS 100LL FUEL DISPENSER WITH FRP UNDER DISPENSER CONTAINMENT

RELOCATED LIGHT POLE ANCHORED TO EMBEDDED ANCHOR BOLTS (1/2"x17"x3")

RELOCATED SPILL KIT

FUEL DISPENSER ISLAND WITH SS FORM AND EMBEDDED ANCHOR BOLTS FOR LIGHT POLE (CONCRETE FILLED, MONOLITHIC POUR) 16'X6'X10"

RELOCATED FUEL HOSE REEL WITH NEW CHAIN, HOSE, SWIVEL AND NOZZLE

FUEL MANAGEMENT PEDESTAL

FUEL HOSE REEL

1
FS.4

EXISTING SECURITY FENCE TO REMAIN (TYP)

TERMINAL BUILDING AND FUEL MANAGERS OFFICE

ENVIRONMENTAL MONITORING CONSOLE IN FUEL MANAGER'S OFFICE

FUEL MANAGED BY COMPUTER IN AIRPORT MANAGER'S OFFICE

EXISTING FUEL SYSTEM POWER AND CONTROL EQUIPMENT

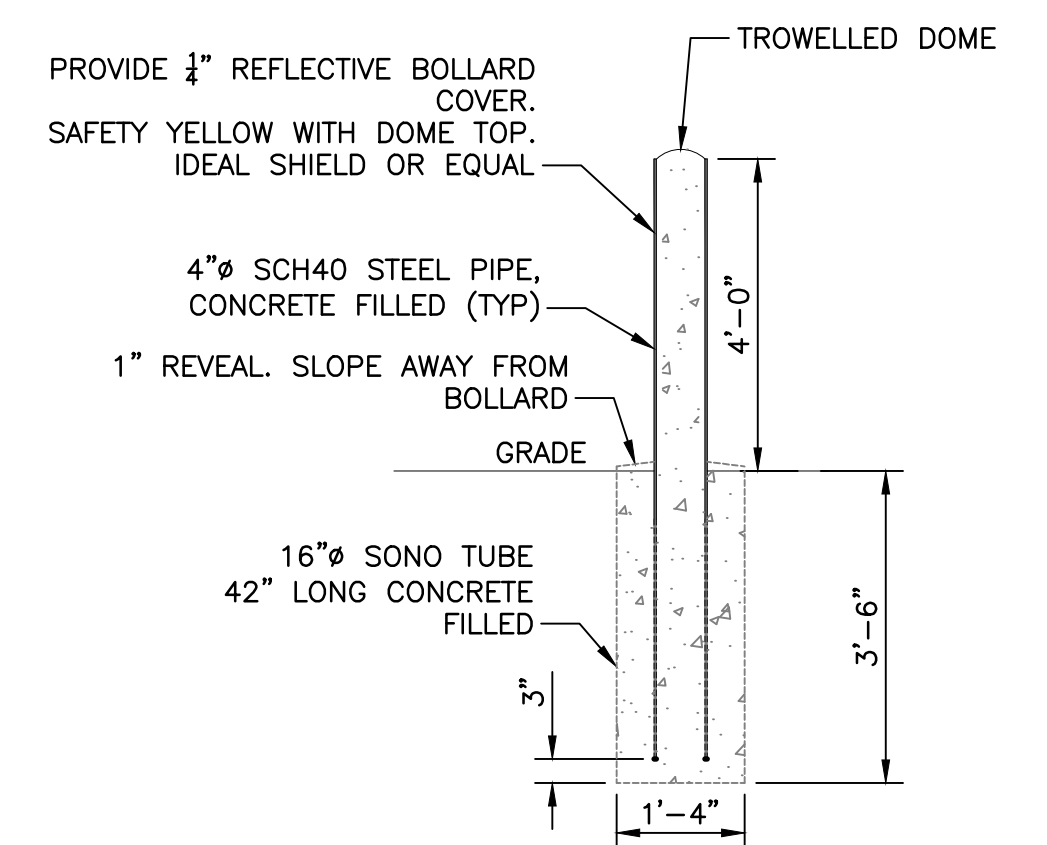
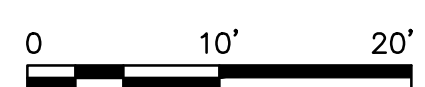
EXISTING SNOW REMOVAL EQUIPMENT (SRE) BUILDING

EXISTING E-STOP BUTTON

COMMUNICATIONS, INTRINSICALLY SAFE, AND (2) 1" SPARE CONDUITS FROM FUEL MANAGER'S OFFICE TO SRE BUILDING REFER TO FS.6

PHASING NOTE:
 THE PROPOSED FUEL SYSTEM EQUIPMENT INSTALLATION IS PART OF PHASE 1 OF THIS PROJECT. REFER TO SHEET C.002 FOR ADDITIONAL INFORMATION.

1 PROPOSED EQUIPMENT PLAN
 SCALE: 1" = 10'



2 BOLLARD DETAIL
 SCALE: NTS

Revision	By	Appd.	MM.DD.YY	Issued	By	Appd.	YY.MM.DD

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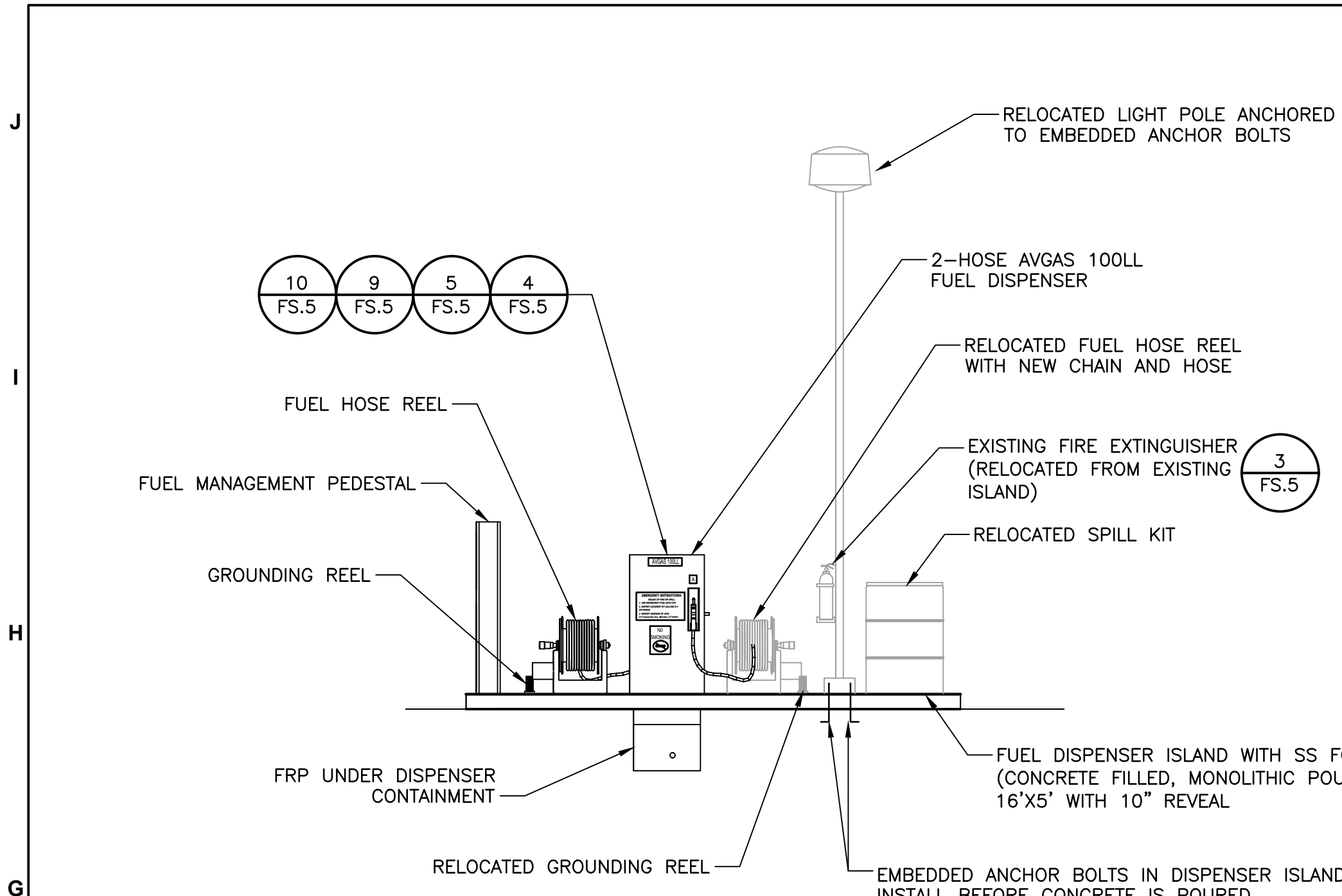
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 CONNECTICUT PE #22581 EXP. 1/31/20

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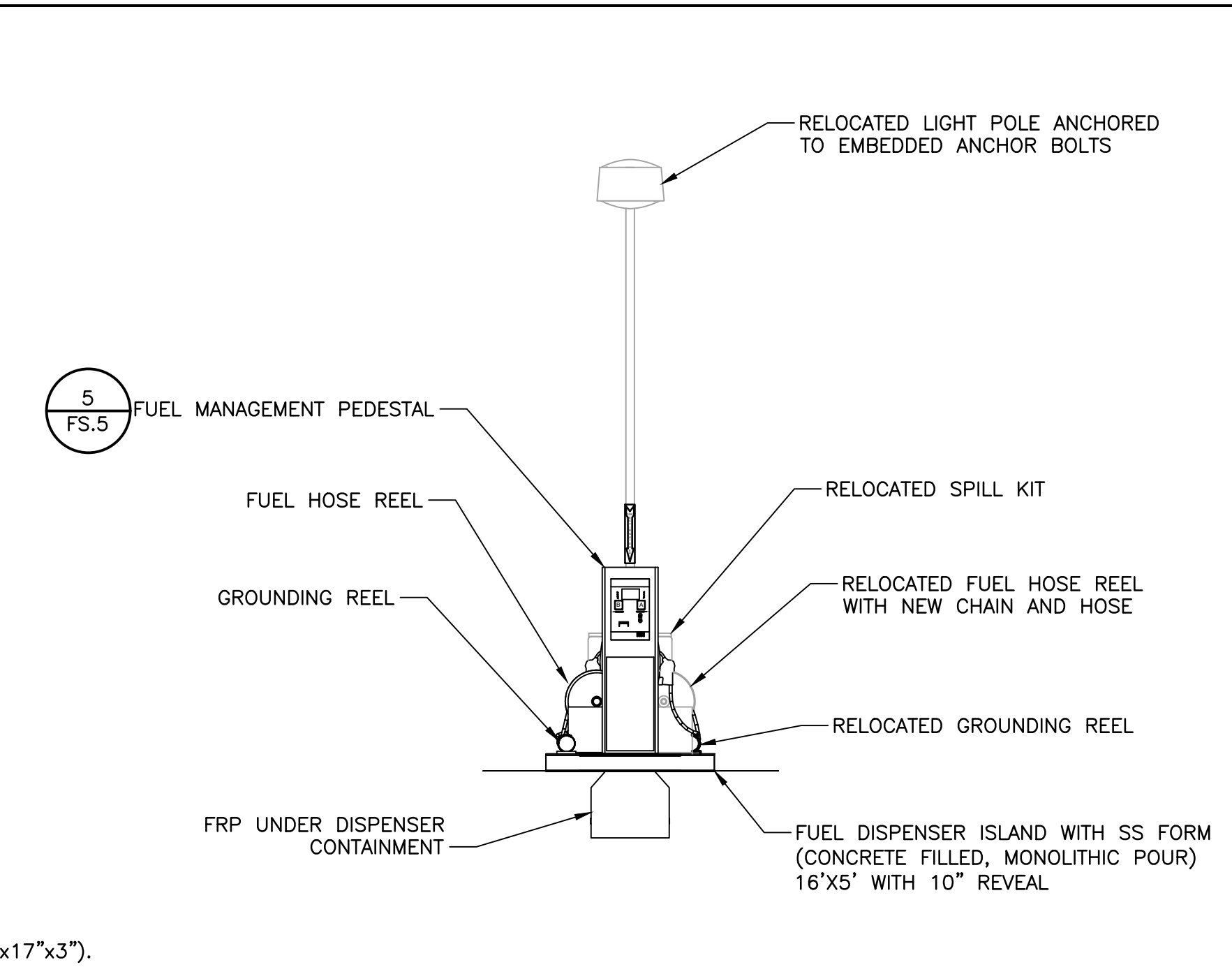
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	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
DRW:	NRD
CHKD:	NIM
DSGN:	NRD
	2019.05.09
	YY.MM.DD

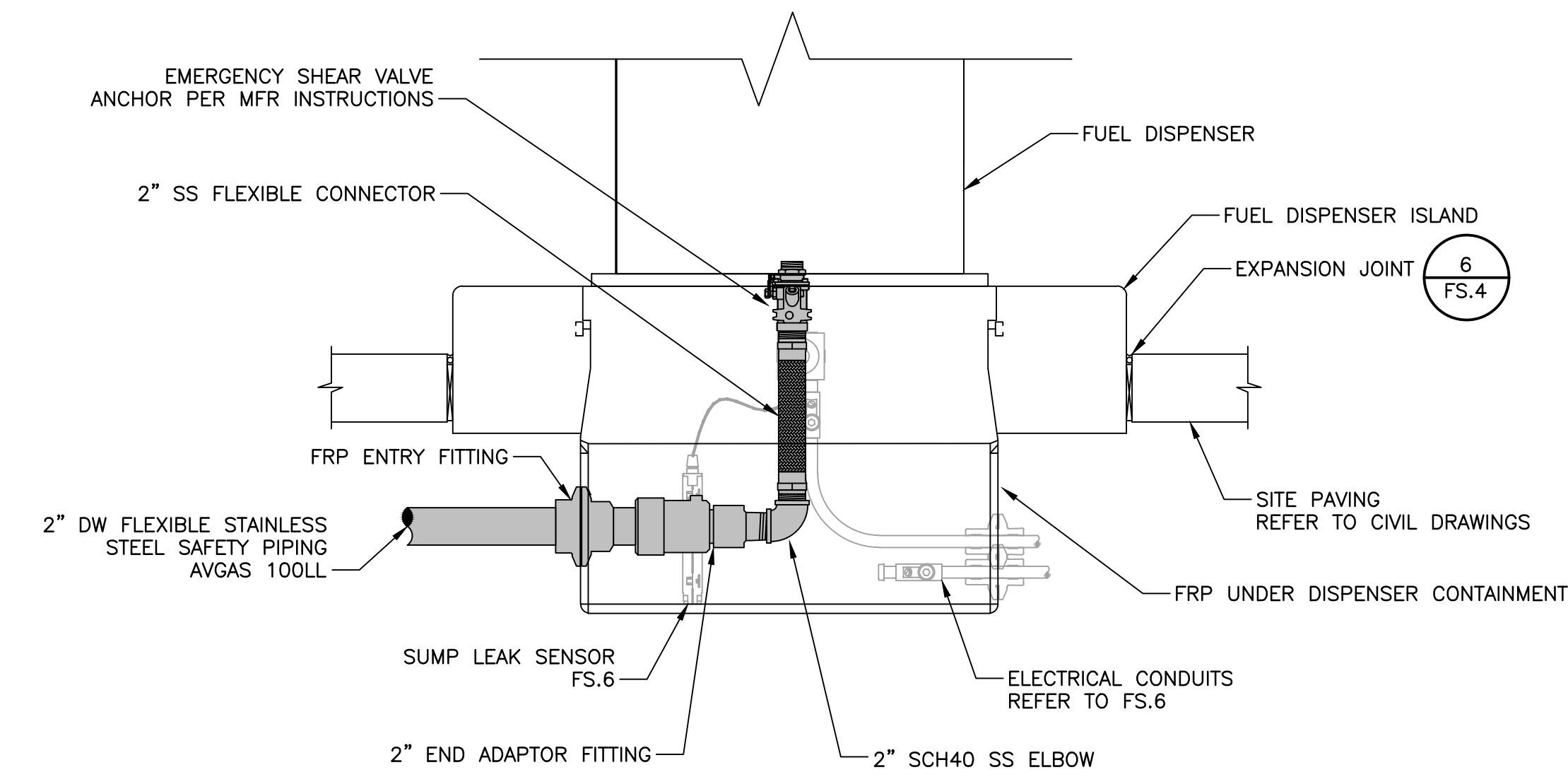
Title		
FUEL SYSTEM PROPOSED EQUIPMENT PLAN (PHASE 1)		
Project No.	Scale	
195211190	AS NOTED	
Drawing No.	Sheet	Revision
FS.3	16 of 23	0



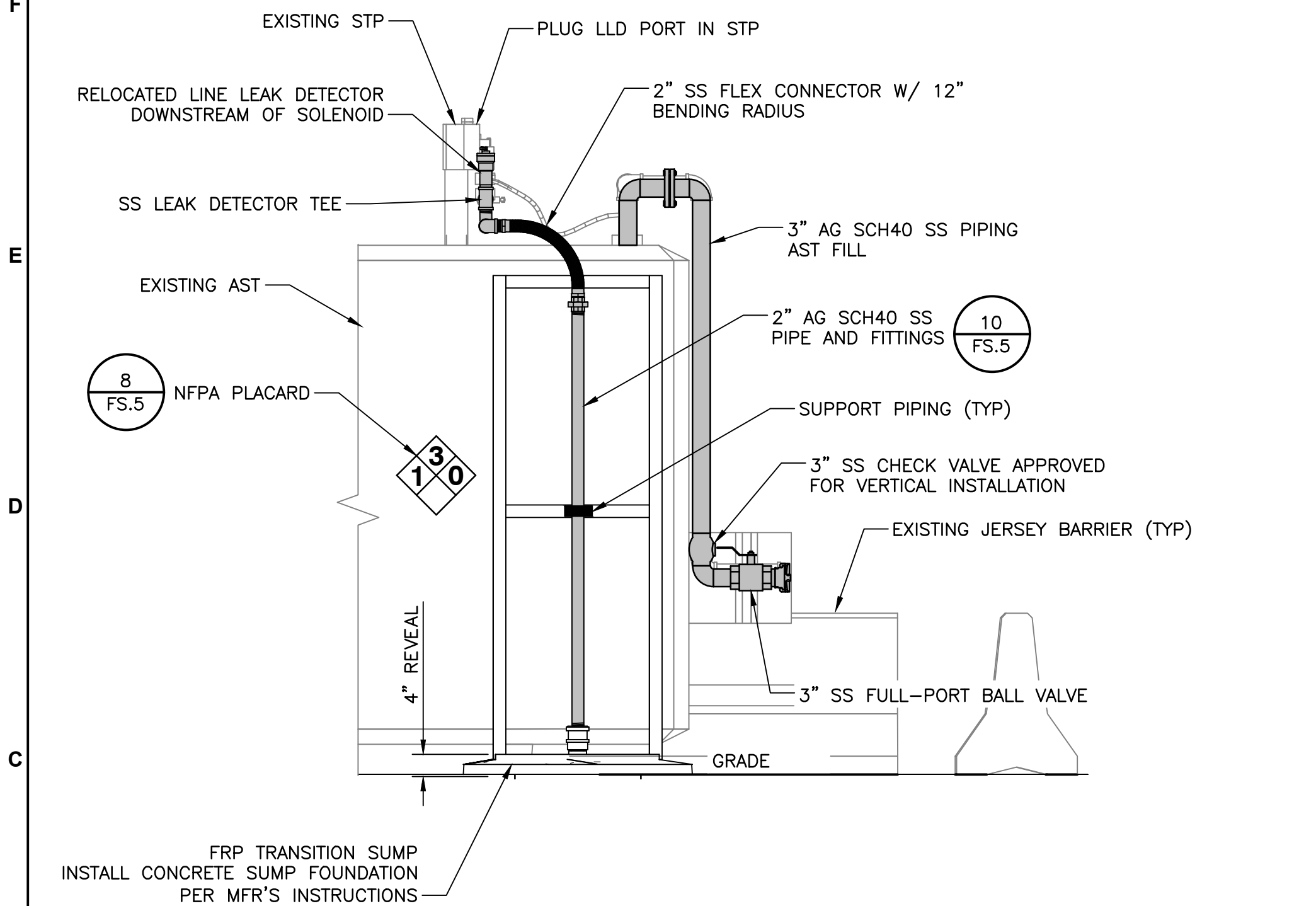
1 FUEL ISLAND ELEVATION
SCALE: 1" = 4'



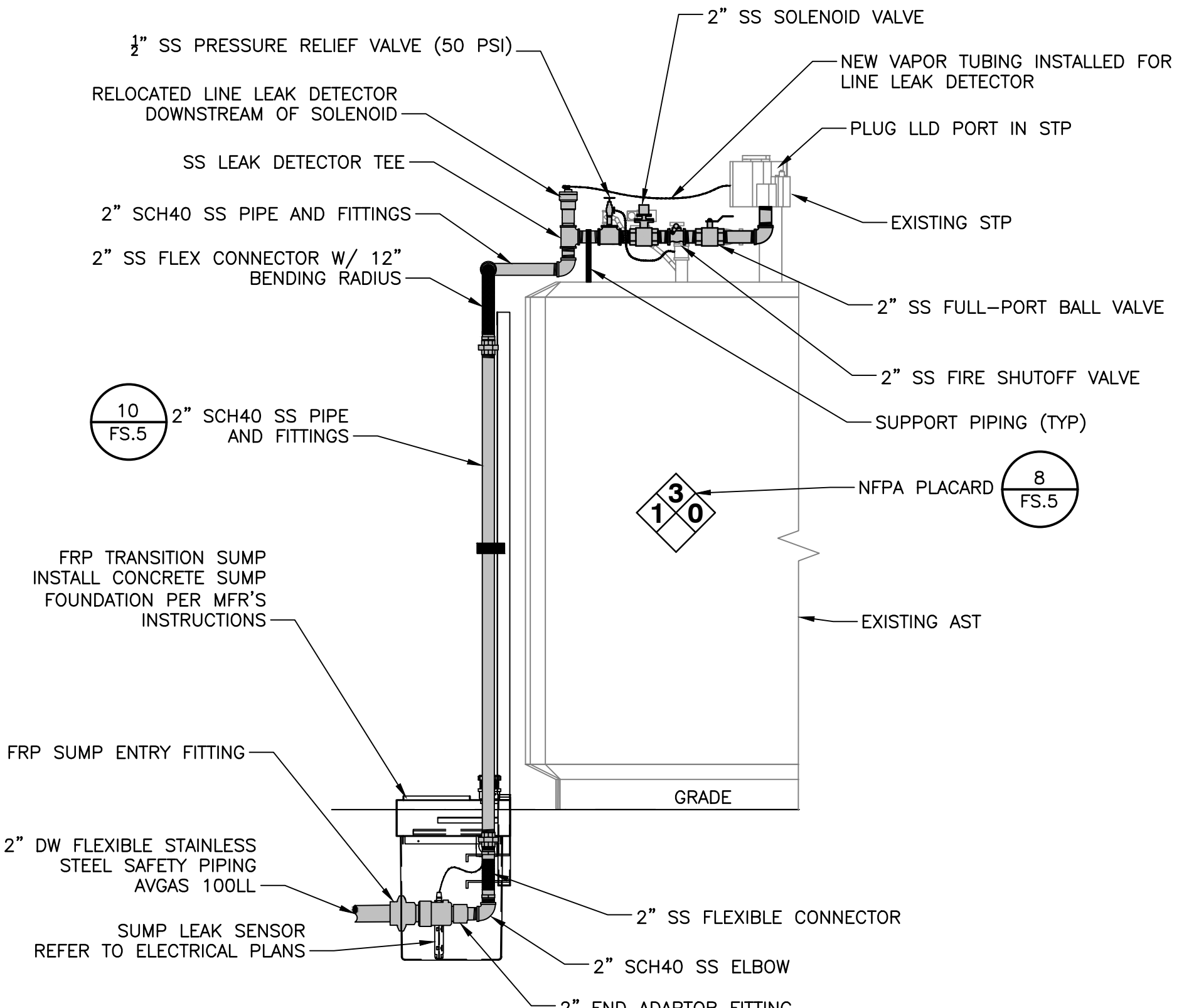
2 FUEL ISLAND ELEVATION
SCALE: 1" = 4'



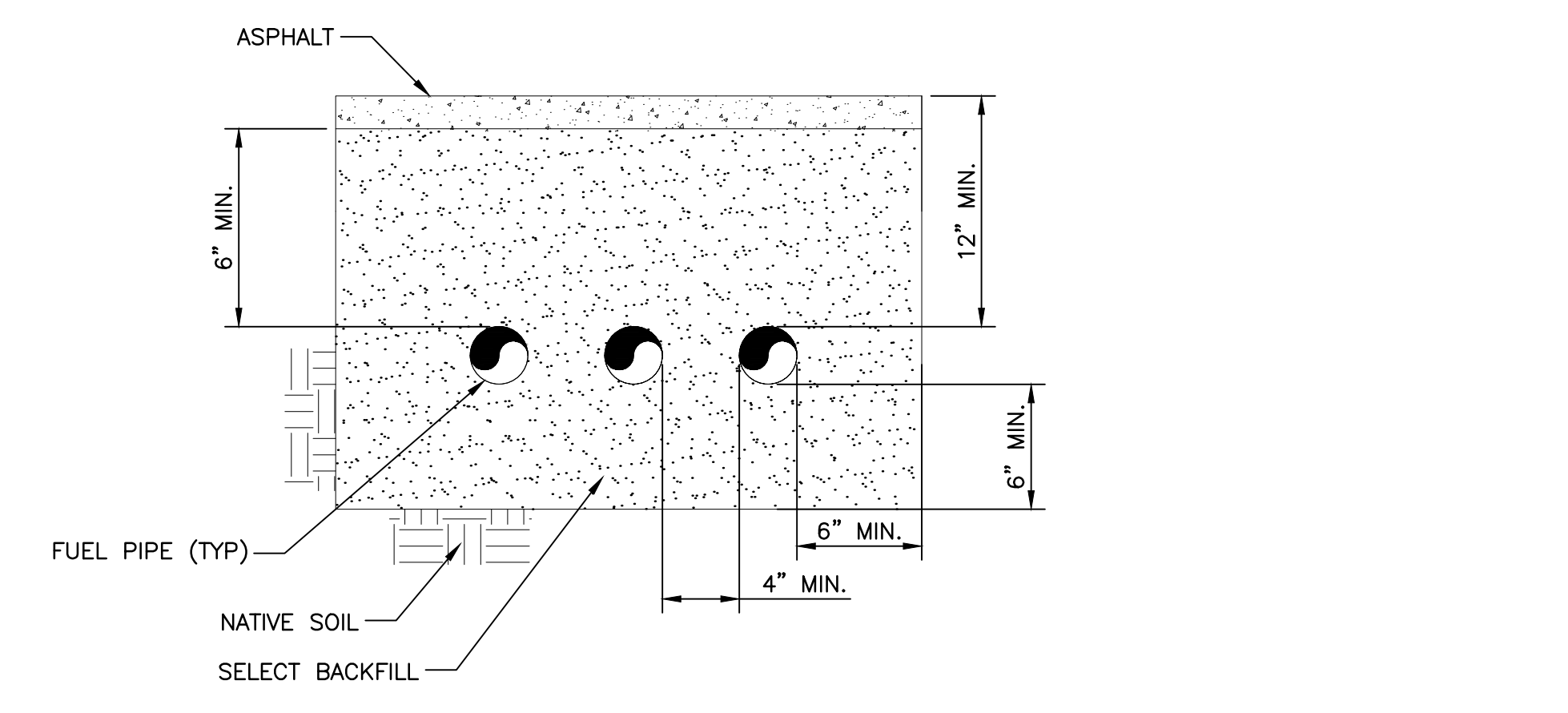
5 FUEL DISPENSER PIPING DETAIL
SCALE: 1" = 1'



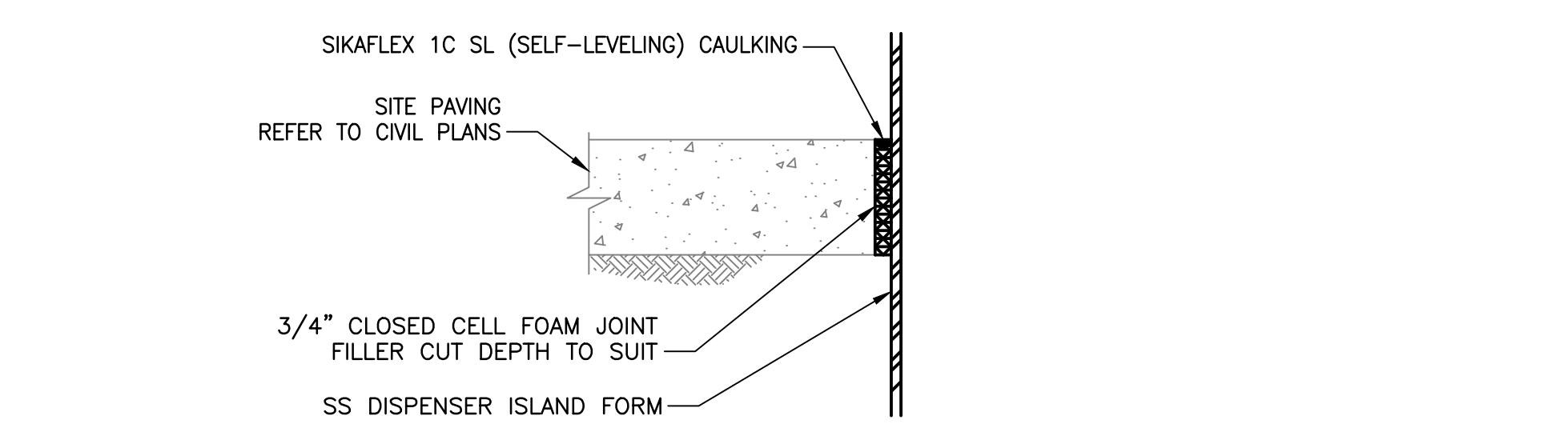
3 AST PIPING ELEVATION
SCALE: 1" = 2'



4 AST PIPING ELEVATION
SCALE: 1" = 2'



6 UG FUEL PIPING TRENCH DETAIL
SCALE: NTS



7 EXPANSION JOINT DETAIL
SCALE: NTS

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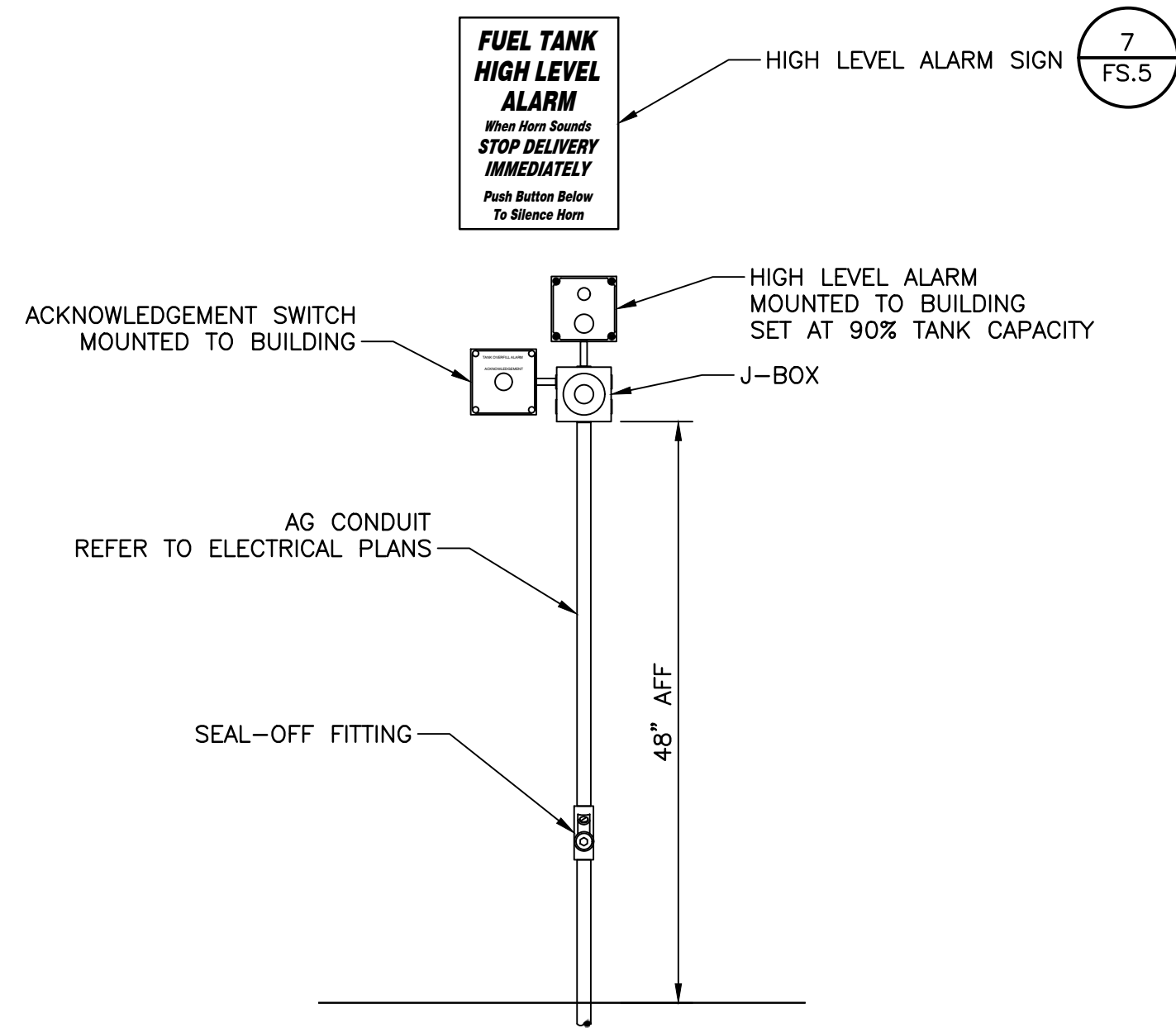
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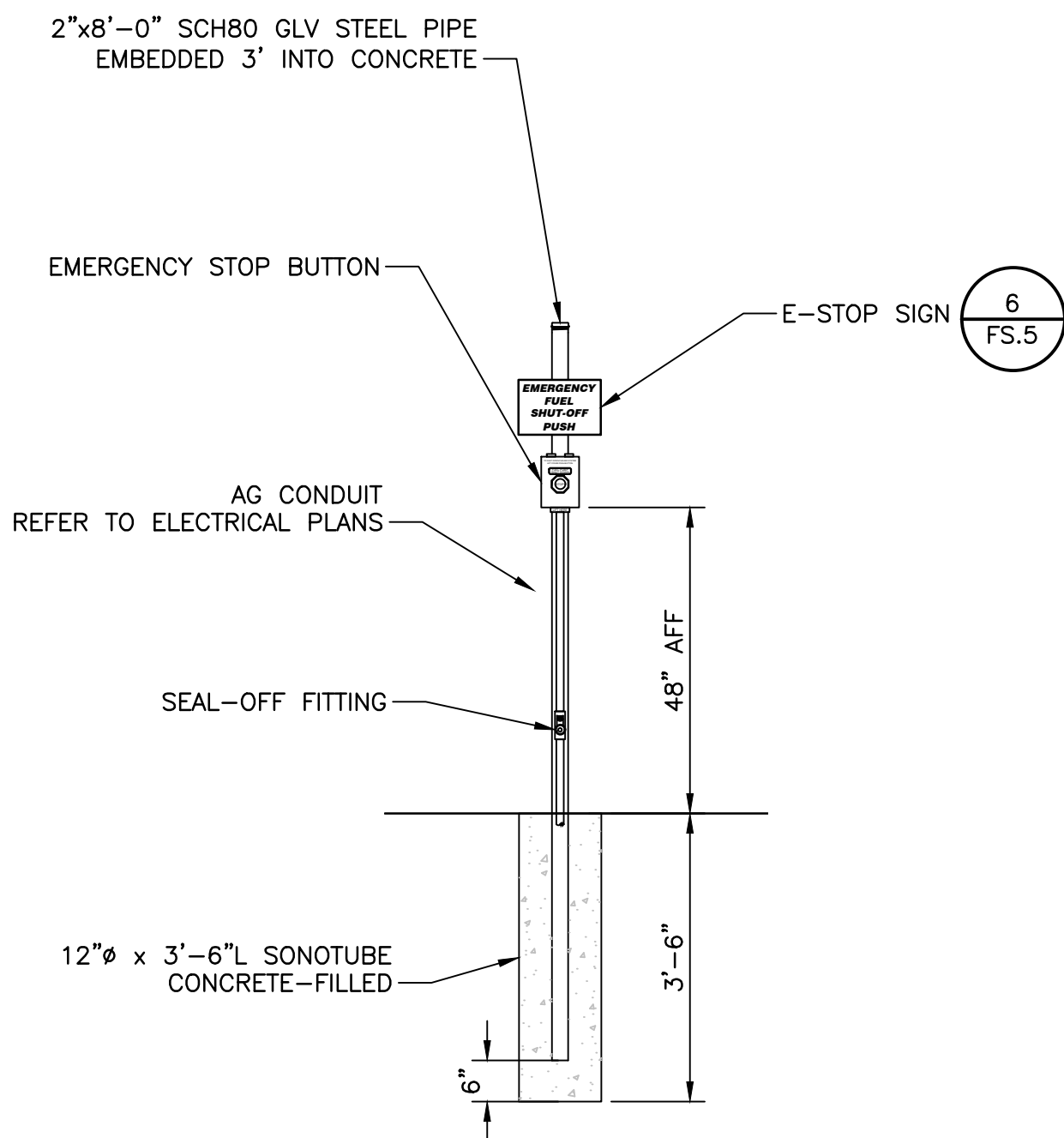
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	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
DRW:	NRD
CHKD:	NTM
DSGN:	NRD
	2019.05.09
	YY.MM.DD

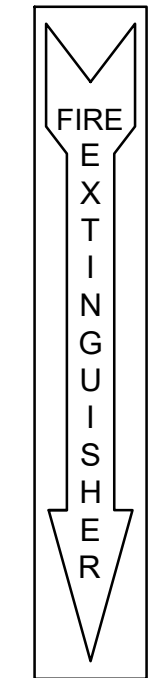
Title	
FUEL SYSTEM SECTIONS AND DETAILS 1 OF 2	
Project No.	Scale
195211190	AS NOTED
Drawing No.	Sheet
FS.4	18 of 23
Revision	
	0



1 HIGH LEVEL ALARM DETAIL
SCALE: 1" = 1'



2 E-STOP BUTTON DETAIL
SCALE: 1" = 2'



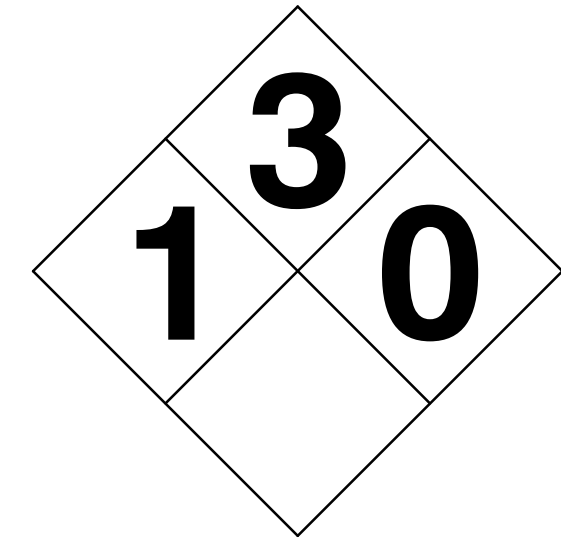
3 FIRE EXTINGUISHER SIGN
SCALE: NTS

EMERGENCY INSTRUCTIONS:
INCASE OF FIRE OR SPILL:
1. USE EMERGENCY FUEL SHUT OFF
2. REPORT ACCIDENT BY CALLING 911 ON PHONE
3. REPORT ADDRESS OF SITE:
213 Evansville Ave, Meriden, CT 06451

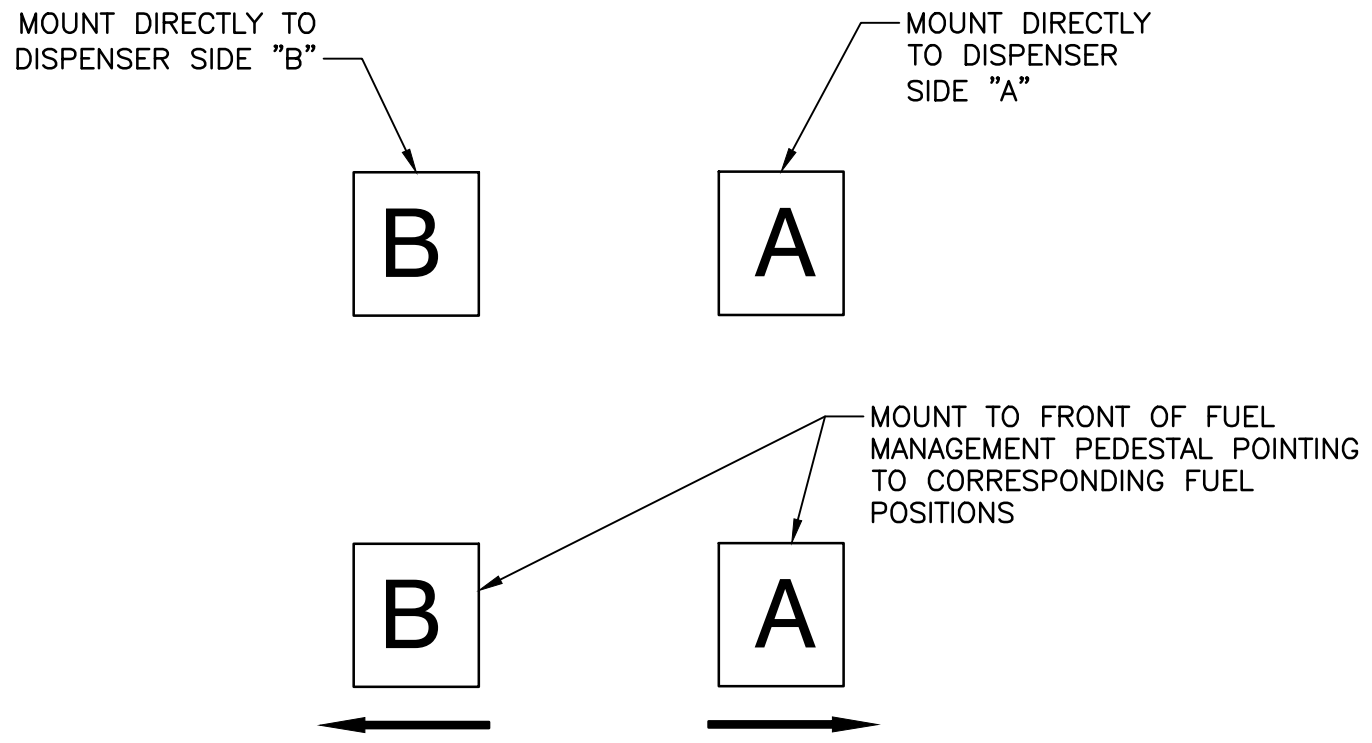
4 EMERGENCY INSTRUCTIONS SIGN
SCALE: NTS

**FUEL TANK
HIGH LEVEL
ALARM**
*When Horn Sounds
STOP DELIVERY
IMMEDIATELY*
**Push Button Below
To Silence Horn**

7 HIGH LEVEL ALARM SIGN
SCALE: NTS



8 NFPA PLACARD DETAIL
SCALE: NTS



5 DISPENSER LABEL DECALS
SCALE: NTS

**EMERGENCY
FUEL
SHUT-OFF
PUSH**

6 E-STOP SIGN
SCALE: NTS



9 HIGH LEVEL ALARM SIGN
SCALE: NTS

AVGAS 100LL

10 AVGAS 100LL LABELING
SCALE: NTS



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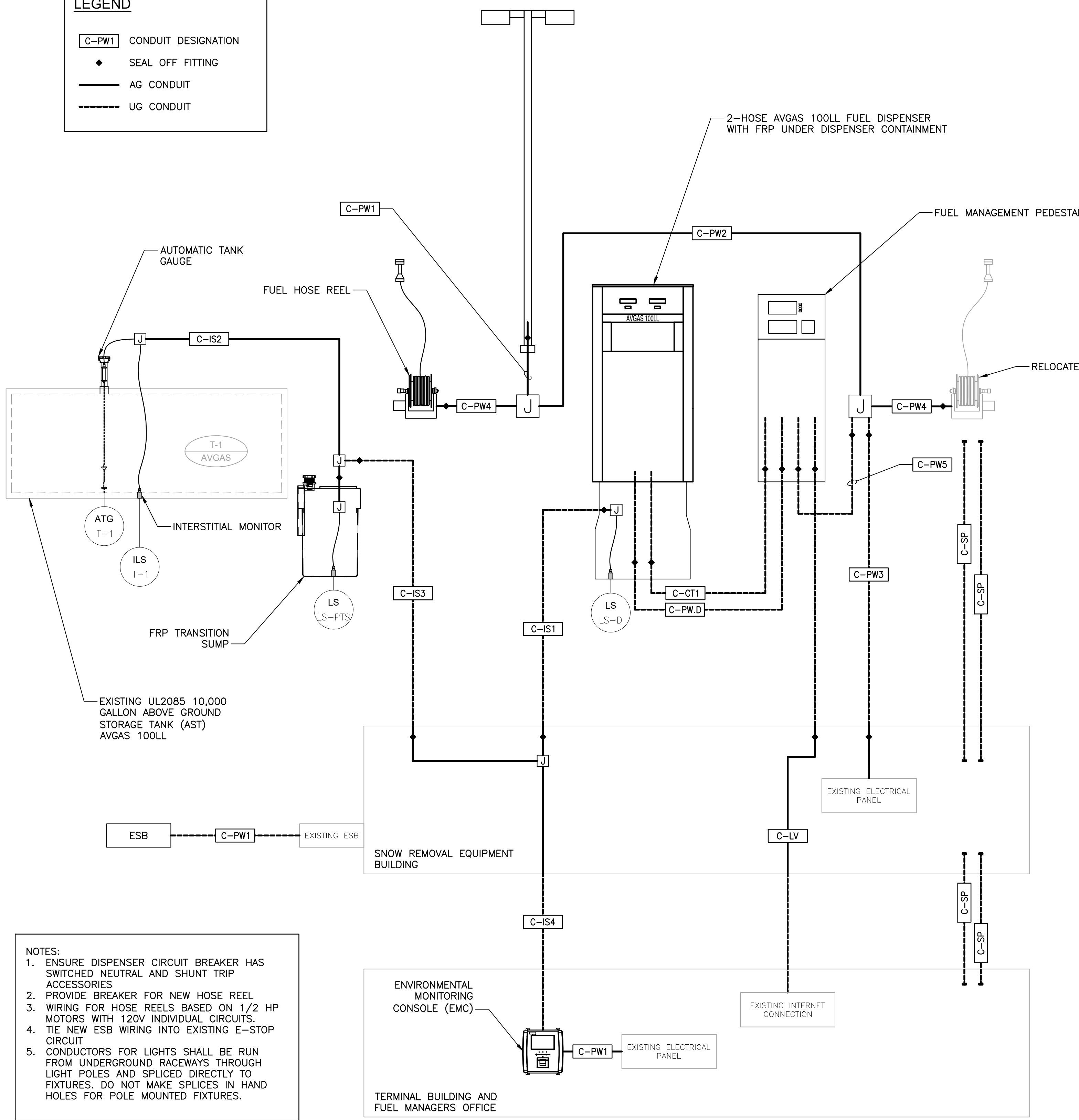
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	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
NRD Dwn.	NRD Chkd.
NRD Dsgn.	2019.05.09 YY.MM.DD

Title		
FUEL SYSTEM SECTIONS AND DETAILS 2 OF 2		
Project No.	Scale	
195211190	AS NOTED	
Drawing No.	Sheet	Revision
FS.5	19 of 23	0

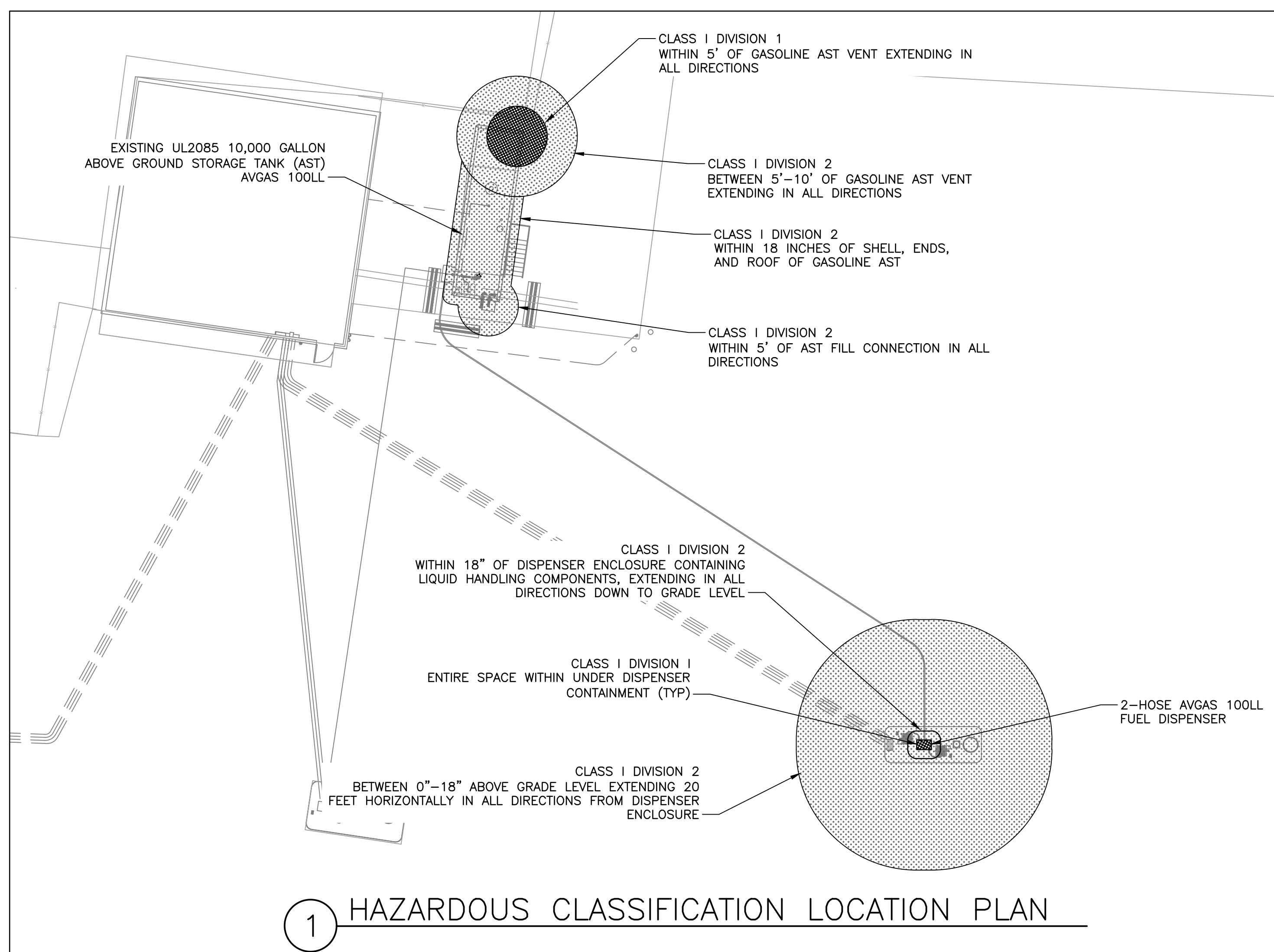
LEGEND

C-PW1	CONDUIT DESIGNATION
◆	SEAL OFF FITTING
—	AG CONDUIT
- - -	UG CONDUIT



- NOTES:**
1. ENSURE DISPENSER CIRCUIT BREAKER HAS SWITCHED NEUTRAL AND SHUNT TRIP ACCESSORIES
 2. PROVIDE BREAKER FOR NEW HOSE REEL
 3. WIRING FOR HOSE REELS BASED ON 1/2 HP MOTORS WITH 120V INDIVIDUAL CIRCUITS.
 4. TIE NEW ESB WIRING INTO EXISTING E-STOP CIRCUIT
 5. CONDUCTORS FOR LIGHTS SHALL BE RUN FROM UNDERGROUND RACEWAYS THROUGH LIGHT POLES AND SPLICED DIRECTLY TO FIXTURES. DO NOT MAKE SPLICES IN HAND HOLES FOR POLE MOUNTED FIXTURES.

CONDUIT DESIGNATION	SIZE	DESCRIPTION	CONDUCTORS	NOTES
C-IS1	3/4"	I.S. CONDUIT	1-BELDEN 87761	
C-IS2	3/4"	I.S. CONDUIT	2-BELDEN 87761	
C-IS3	3/4"	I.S. CONDUIT	3-BELDEN 87761	
C-IS4	3/4"	I.S. CONDUIT	4-BELDEN 87761	
C-LV	3/4"	LOW VOLTAGE CONDUIT	1-BELDEN 7931A, 1-BELDEN 89729	COIL 12 in. BELDEN 89729 AT ENDS
C-PW1	3/4"	POWER CONDUIT	2-#12, 1-#12G	MULTIPLE POWER CONDUIT DESIGNATION
C-PW2	3/4"	POWER CONDUIT	2-#8, 2-#12, 1-#10G	CIRCUITS: HOSE REEL; LIGHTING
C-PW3	1 1/4"	POWER CONDUIT	4-#8, 2-#10, 5-#12, 1-#10G	CIRCUITS: HOSE REEL; LIGHTING, DISPENSER, FMS
C-PW4	3/4"	POWER CONDUIT	2-#8, 1-#10G	CIRCUITS: HOSE REEL
C-PW5	3/4"	POWER CONDUIT	2-#10, 3-#12, 1-#10G	CIRCUITS: DISPENSER, FMS
C-PW.D	3/4"	POWER CONDUIT	2-#10, 1-#12, 1-#10G	CIRCUITS: DISPENSER
C-CTRL	1"	DISPENSER CONTROL	5-#12, 2-BELDEN 87761, 1-#12G	CIRCUITS: PULSERS & CONTROL
C-SP	1"	SPARE	EMPTY	



1 HAZARDOUS CLASSIFICATION LOCATION PLAN

Revision	By	Appd.	MM.DD.YY	Issued	By	Appd.	YY.MM.DD

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File Name:	195211190
Dwn.	JSW
Chkd.	NIM
Dsgn.	JSW
	2019.05.09
	YY.MM.DD

Title	
FUEL SYSTEM ELECTRICAL SLD, CONDUIT SCHEDULE AND HAZARDOUS CLASSIFICATION PLAN	
Project No.	Scale
195211190	AS NOTED
Drawing No.	Sheet
FS.6	20 of 23
	Revision
	0

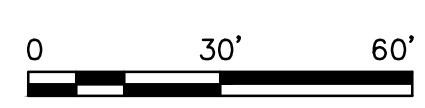
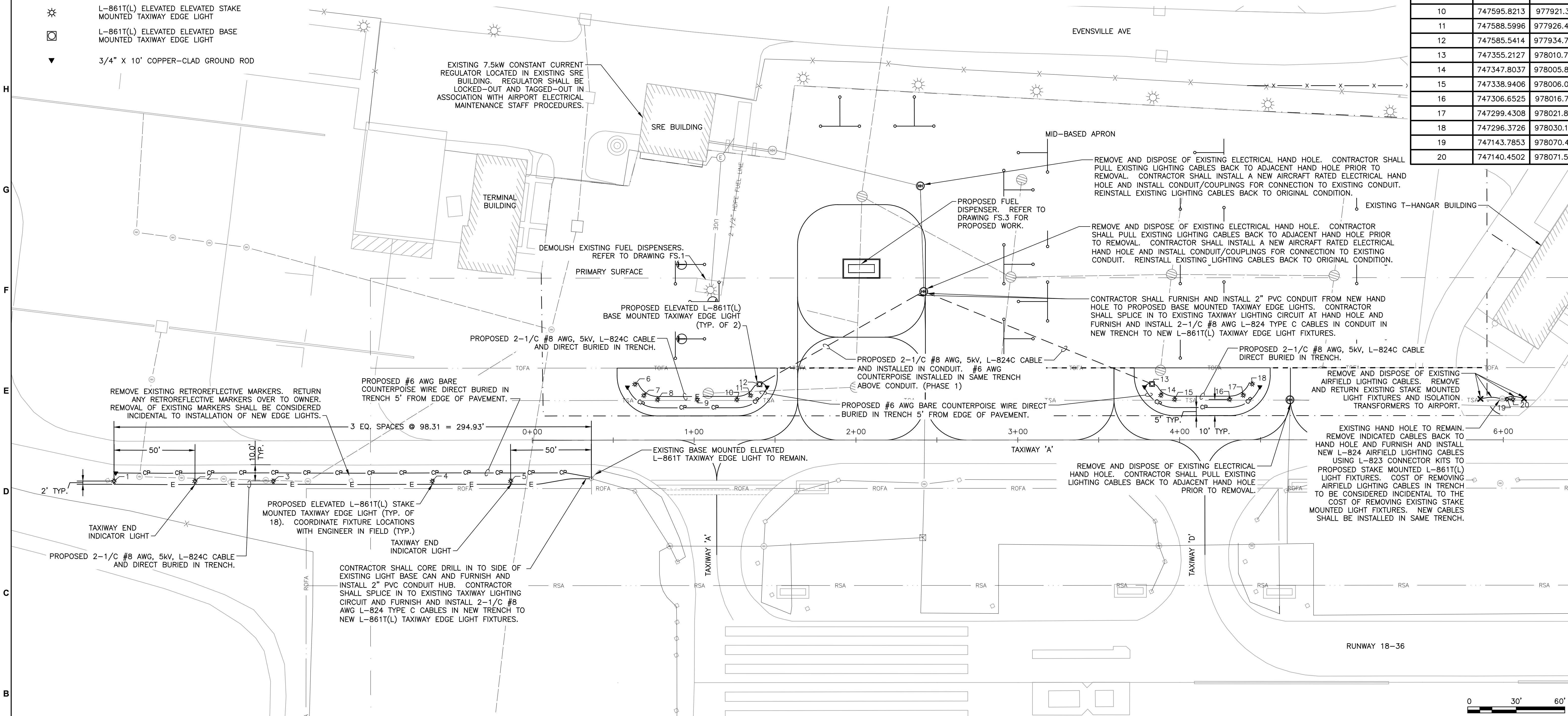
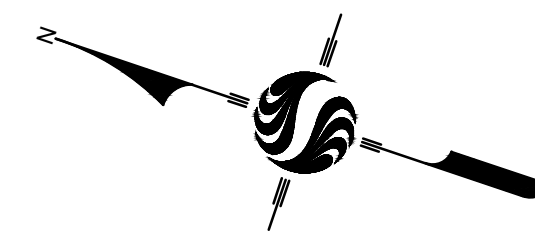
LEGEND

- L-824 #8 AWG TYPE X XKV AIRFIELD LIGHTING CABLE AND #6 AWG GROUND IN 2" PVC CONDUIT DIRECT BURIED IN TRENCH.
- #6 BARE COUNTERPOISE WIRE RUN IN SAME TRENCH LOCATED 4" ABOVE CONDUIT.
- L-824 #8 AWG TYPE 'C', 5KV AIRFIELD LIGHTING CABLE AND DIRECT BURIED IN TRENCH.
- #6 AWG BARE COUNTERPOISE WIRE DIRECT BURIED IN TRENCH.
- ⊕ PROPOSED ELECTRICAL HANDHOLE
- ⊛ L-861T(L) ELEVATED ELEVATED STAKE MOUNTED TAXIWAY EDGE LIGHT
- ⊠ L-861T(L) ELEVATED BASE MOUNTED TAXIWAY EDGE LIGHT
- ▼ 3/4" X 10' COPPER-CLAD GROUND ROD

NOTES:

1. ALL UNDERGROUND UTILITIES SHOWN ON THE DRAWINGS ARE SHOWN IN THEIR APPROXIMATE LOCATIONS ONLY. ADDITIONAL UNDERGROUND UTILITIES OTHER THAN THOSE SHOWN ON THESE DRAWINGS MAY EXIST. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MARKING THE EXACT LOCATION AND DEPTH OF ALL UNDERGROUND UTILITIES PRIOR TO ANY EXCAVATION. HAND DIGGING SHALL BE PERFORMED AS NECESSARY TO AVOID DAMAGE TO UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE TO REPAIR DAMAGED UTILITIES IMMEDIATELY AT NO ADDITIONAL COST TO THE OWNER.
2. CABLE TAGS MUST BE INSTALLED IN HAND HOLES AND MANHOLES IMMEDIATELY FOLLOWING THE PULLING OF CABLE THROUGH THESE STRUCTURES.
3. EXACT LOCATION OF NEW CONDUIT SHALL BE APPROVED BY ENGINEER PRIOR TO INSTALLATION.

TAXIWAY EDGE LIGHTS		
LIGHT NUMBER	NORTHING	EASTING
1	747945.6431	977753.1391
2	747898.1251	977768.6952
3	747852.2371	977783.9597
4	747758.7993	977814.6848
5	747712.8719	977829.8305
6	747658.6263	977910.6643
7	747651.2173	977905.7972
8	747642.3541	977905.9630
9	747619.0877	977913.6391
10	747595.8213	977921.3151
11	747588.5996	977926.4560
12	747585.5414	977934.7764
13	747355.2127	978010.7664
14	747347.8037	978005.8993
15	747338.9406	978006.0652
16	747306.6525	978016.7176
17	747299.4308	978021.8585
18	747296.3726	978030.1790
19	747143.7853	978070.4197
20	747140.4502	978071.5087



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Client/Project
MERIDEN MARKHAM MUNICIPAL AIRPORT

MID-BASE APRON REHABILITATION

MERIDEN, CONNECTICUT

File Name: 195211190

HAB	MLC	HAB	2019.05.09
Dwn.	Chkd.	Dsgn.	YY.MM.DD

Title
ELECTRICAL PLAN

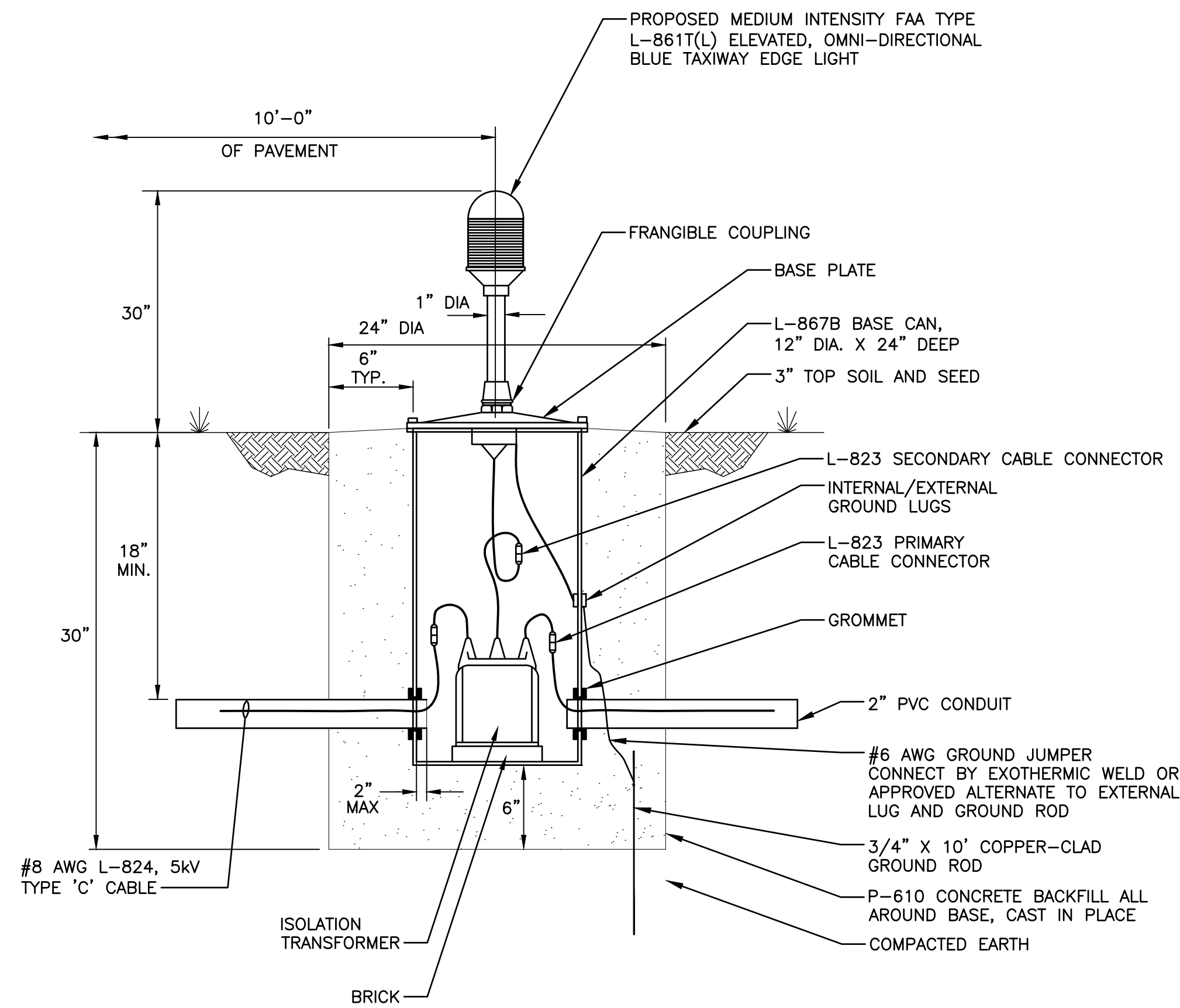
Project No. 195211190

Scale 1" = 30'

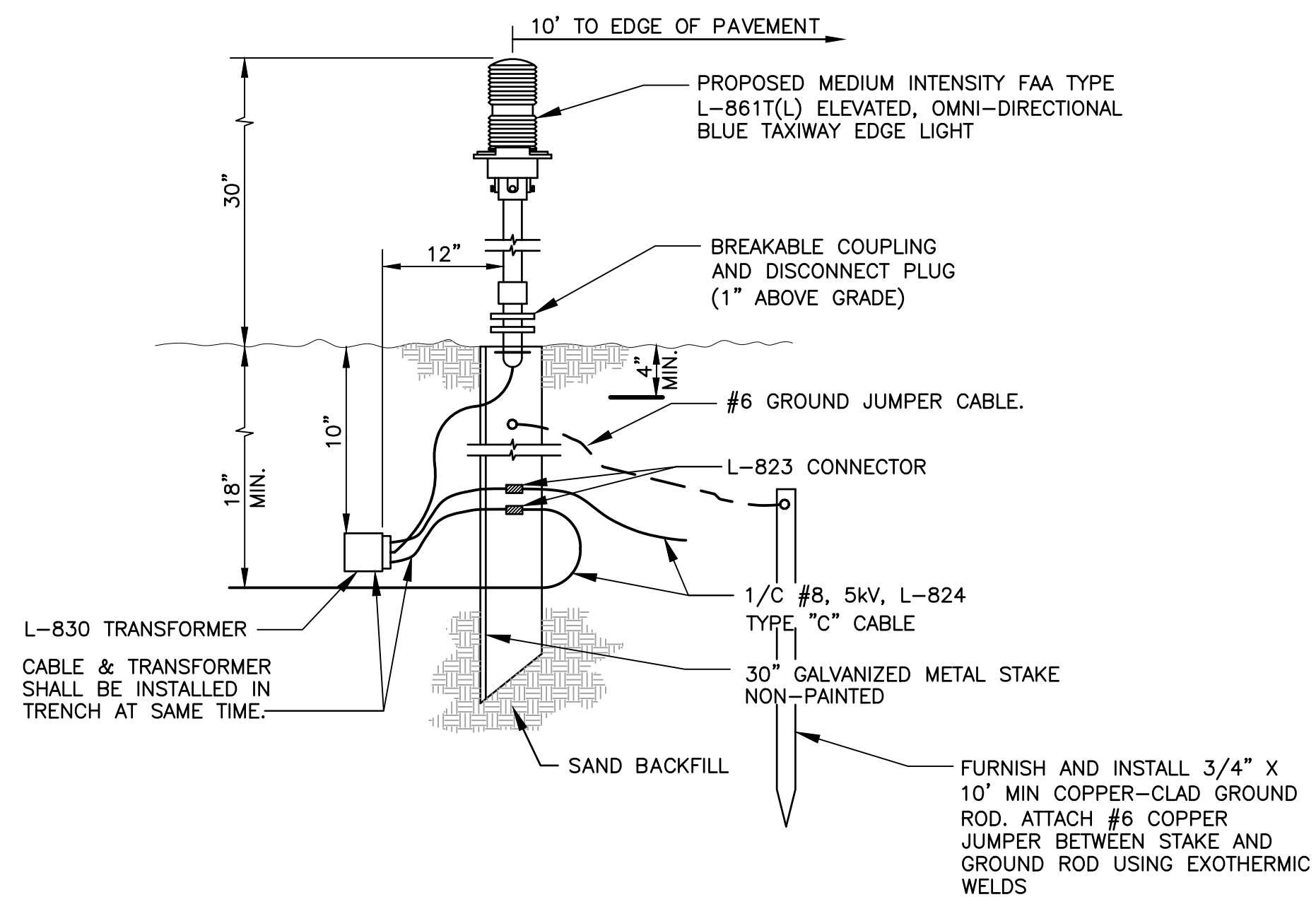
Drawing No. E.200

Sheet 21 of 23

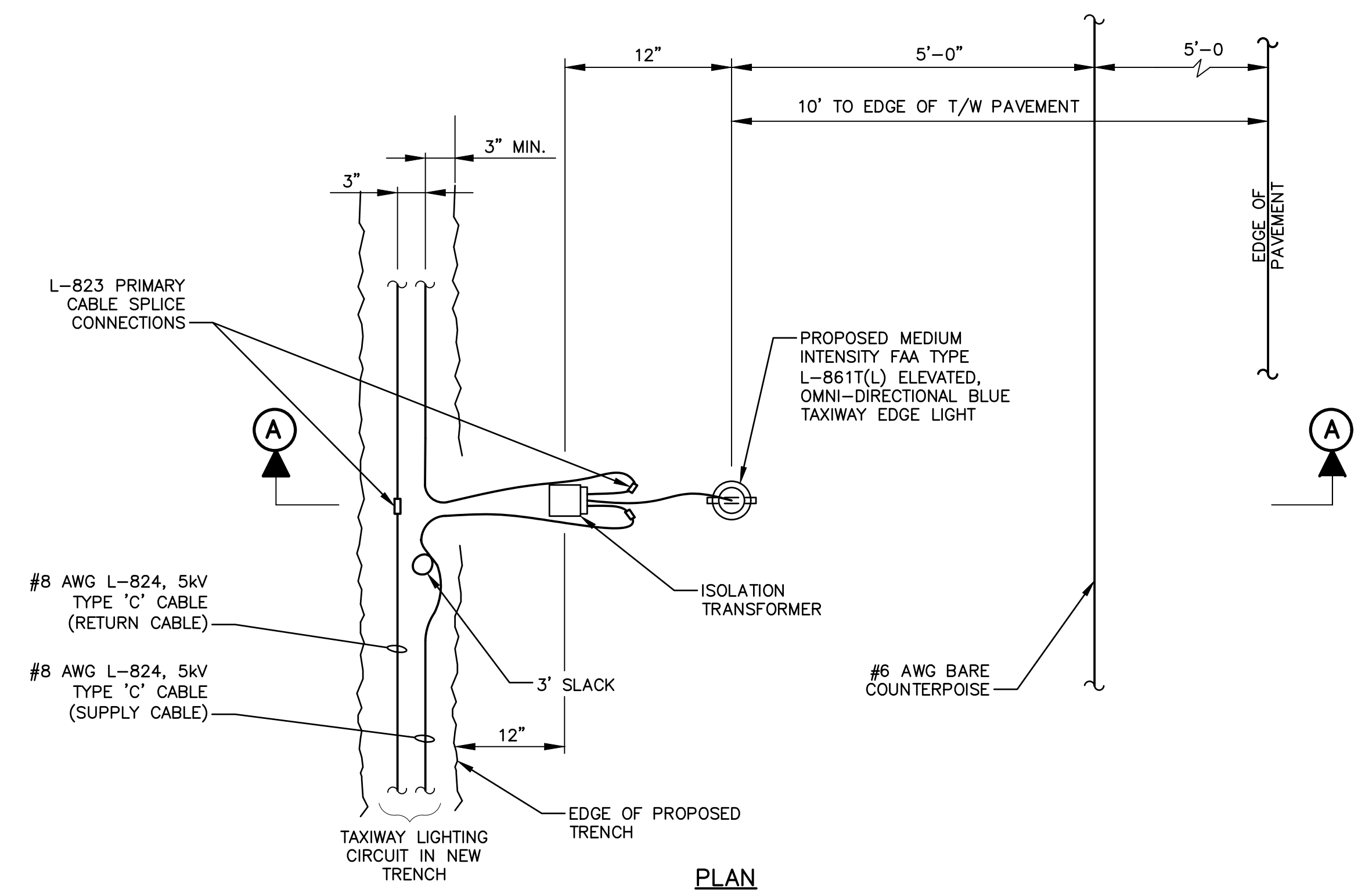
Revision 0



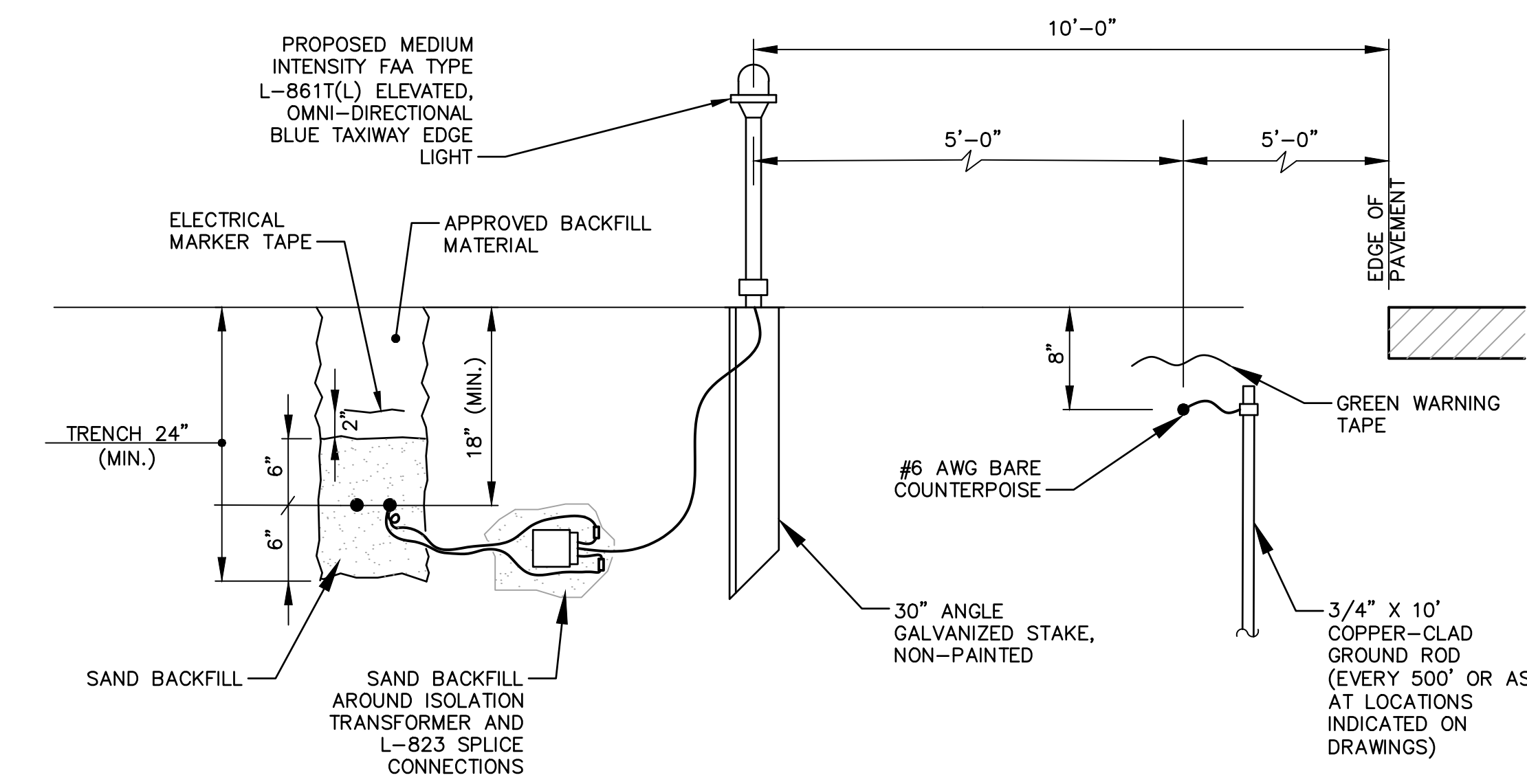
1 L-861T(L) BASE CAN MOUNTED
NOT TO SCALE



2 L-861T(L) STAKE MOUNTED
NOT TO SCALE



PLAN



ELEVATION A-A

NOTE: THIS DETAIL SHALL BE REFERENCED FOR L-824 CABLE INSTALLATION AND STAKE MOUNTED L-861T(L) FIXTURE INSTALLATIONS.

3 STAKE MOUNTED FIXTURE AND CABLE INSTALLATION DETAIL
NOT TO SCALE

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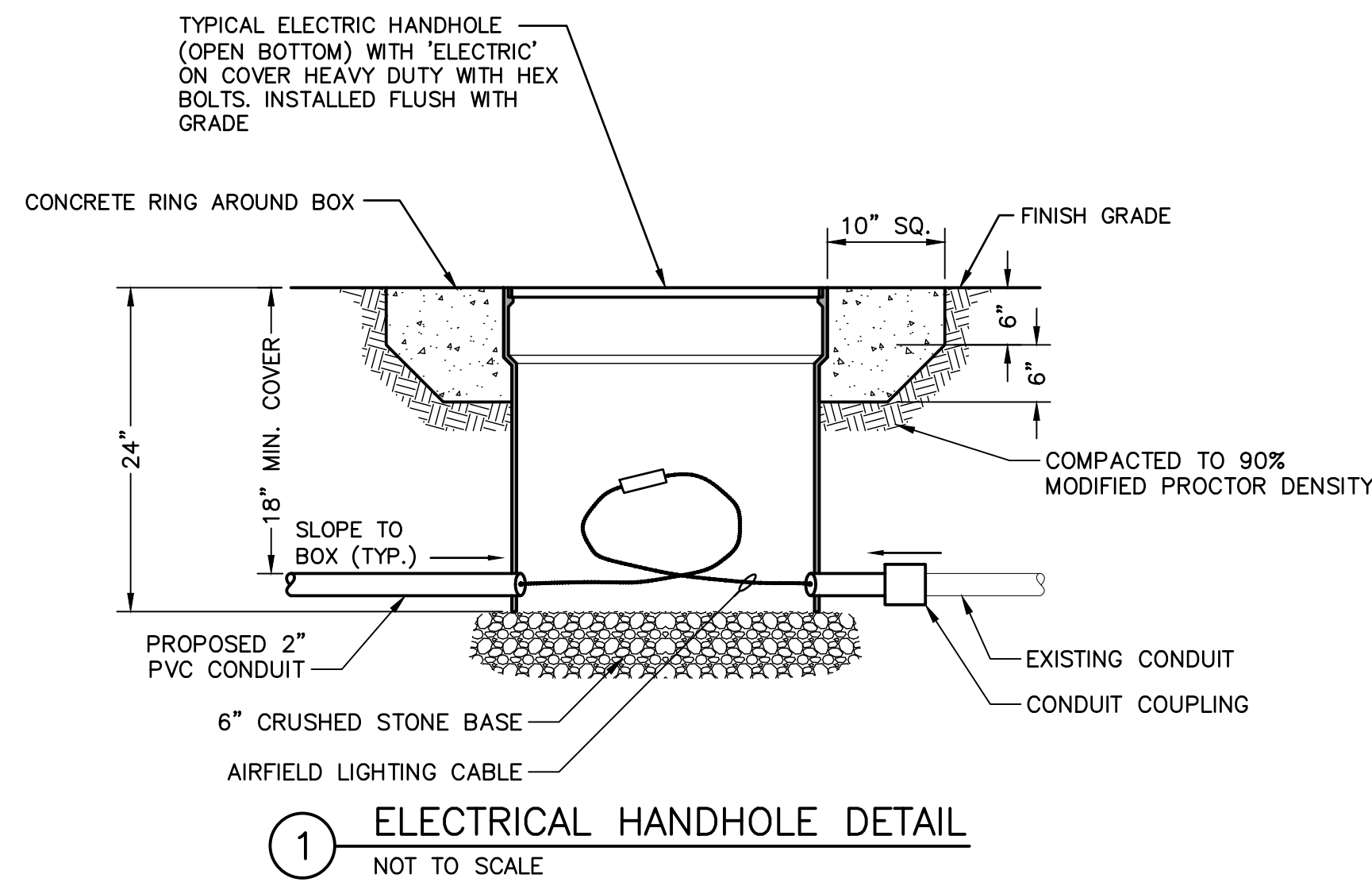


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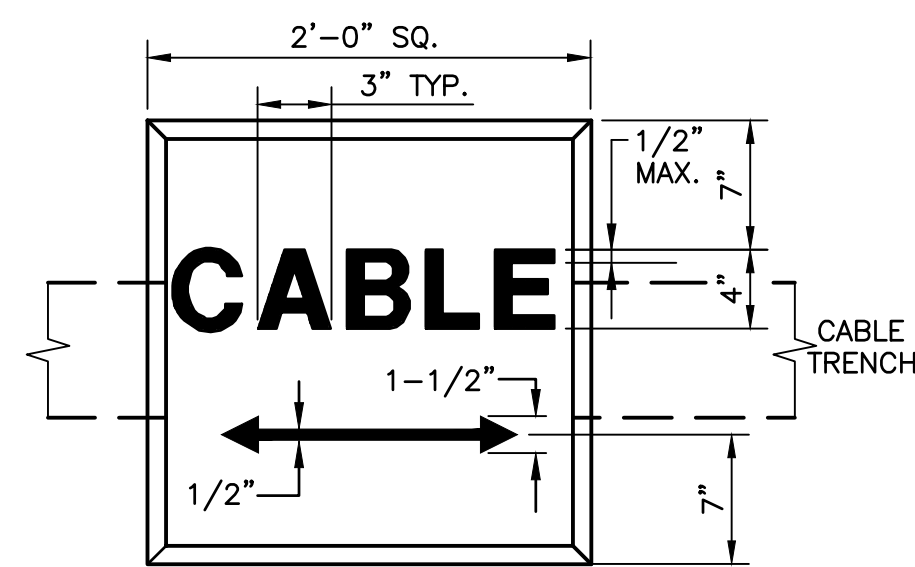
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	MID-BASE APRON REHABILITATION
	MERIDEN, CONNECTICUT
File Name:	195211190
HAB Dwn.	MLC Chkd.
HAB Dsgn.	2019.05.09
	YY.MM.DD

Title	
ELECTRICAL DETAILS - 1	
Project No.	Scale
195211190	AS SHOWN
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	0

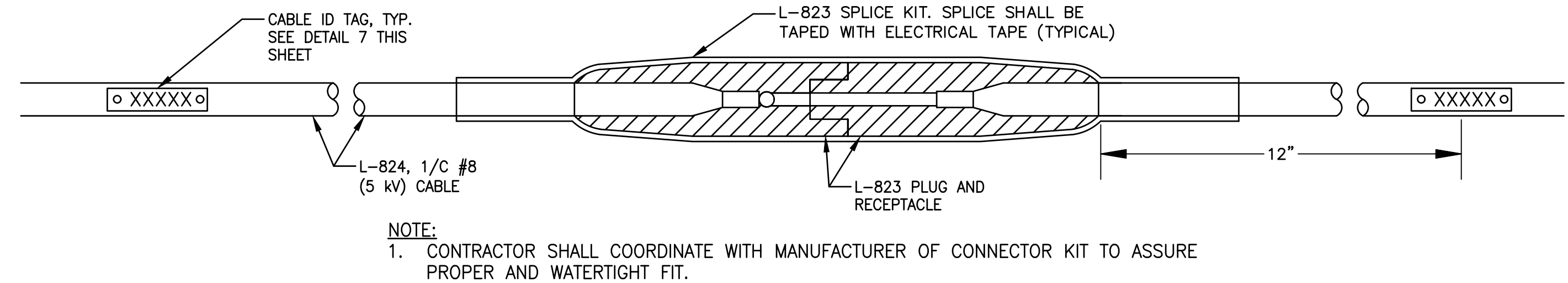


NOTE: SEE ELECTRICAL PLAN DRAWING FOR PROPOSED HAND HOLE LOCATIONS. CONTRACTOR SHALL IDENTIFY WHICH HAND HOLES REQUIRE CONNECTION TO EXISTING CONDUIT AND WHERE PROPOSED CONDUIT NEEDS TO BE FURNISHED AND INSTALLED.

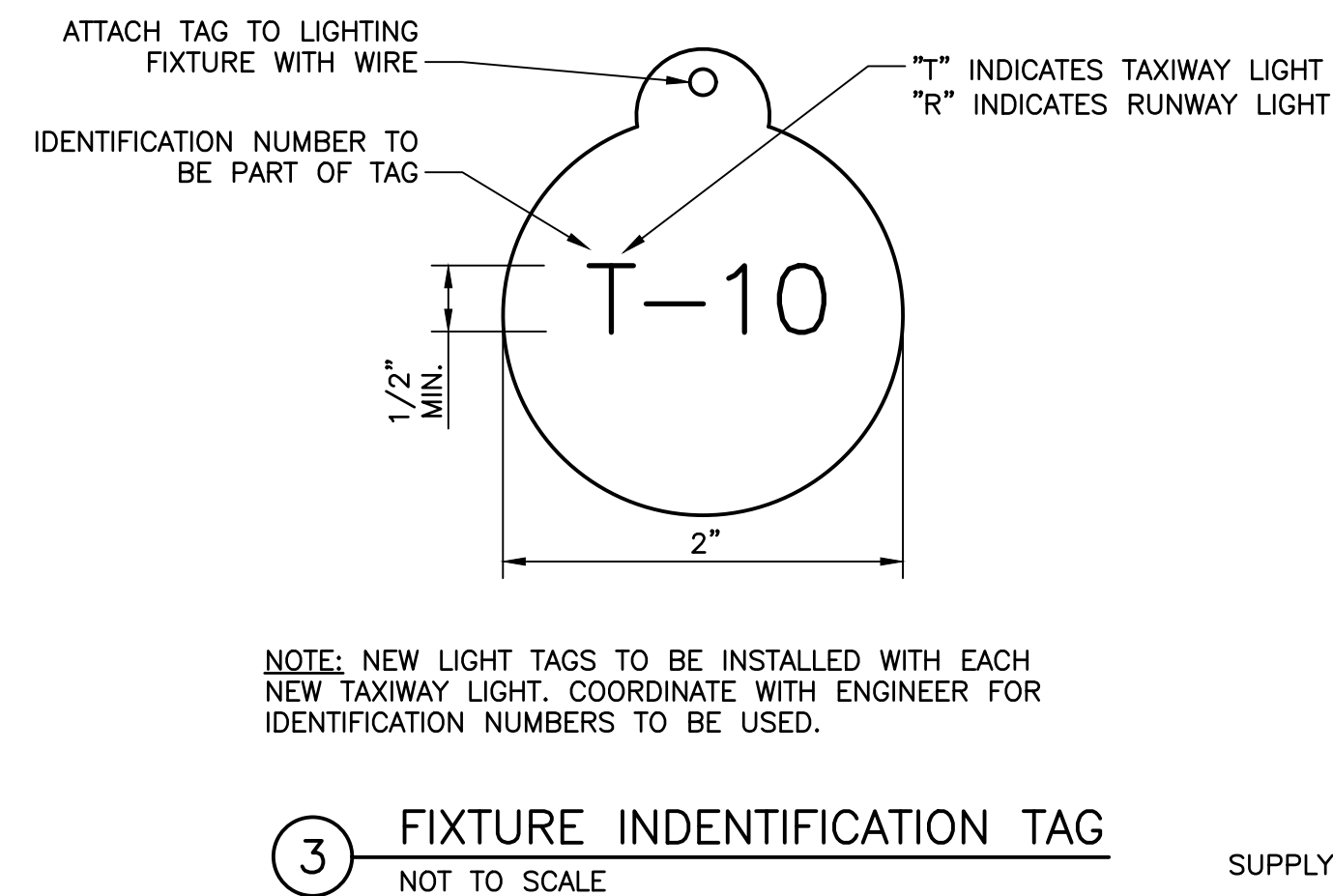


CABLE MARKER NOTES:

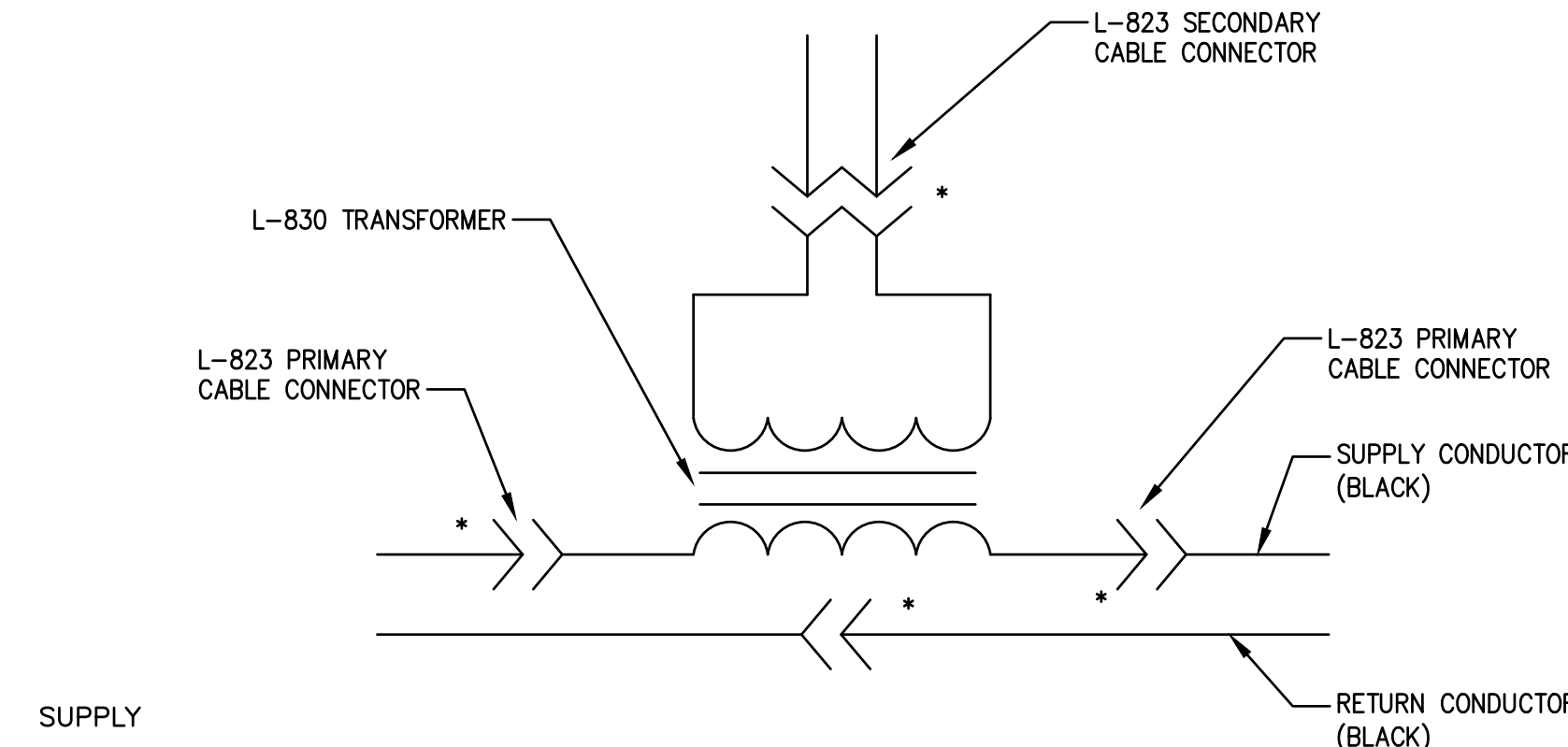
- HAND LETTERING NOT ALLOWED ON MARKERS. LETTERING IS TO BE IMPRESSED.
- ARROW ON CABLE MARKER TO INDICATE DIRECTION OF CABLES. (WHERE APPLICABLE)
- NO SEPARATE PAYMENT WILL BE MADE FOR ANY TYPE OF MARKER.
- ALL CONCRETE MARKERS WILL BE AT LEAST 4" IN THICKNESS.
- CABLE MARKERS WILL BE PLACED AT CHANGE OF DIRECTION AND AS REQUIRED BY ENGINEER.
- MARKER SHALL BE PAINTED INTERNATIONAL ORANGE.



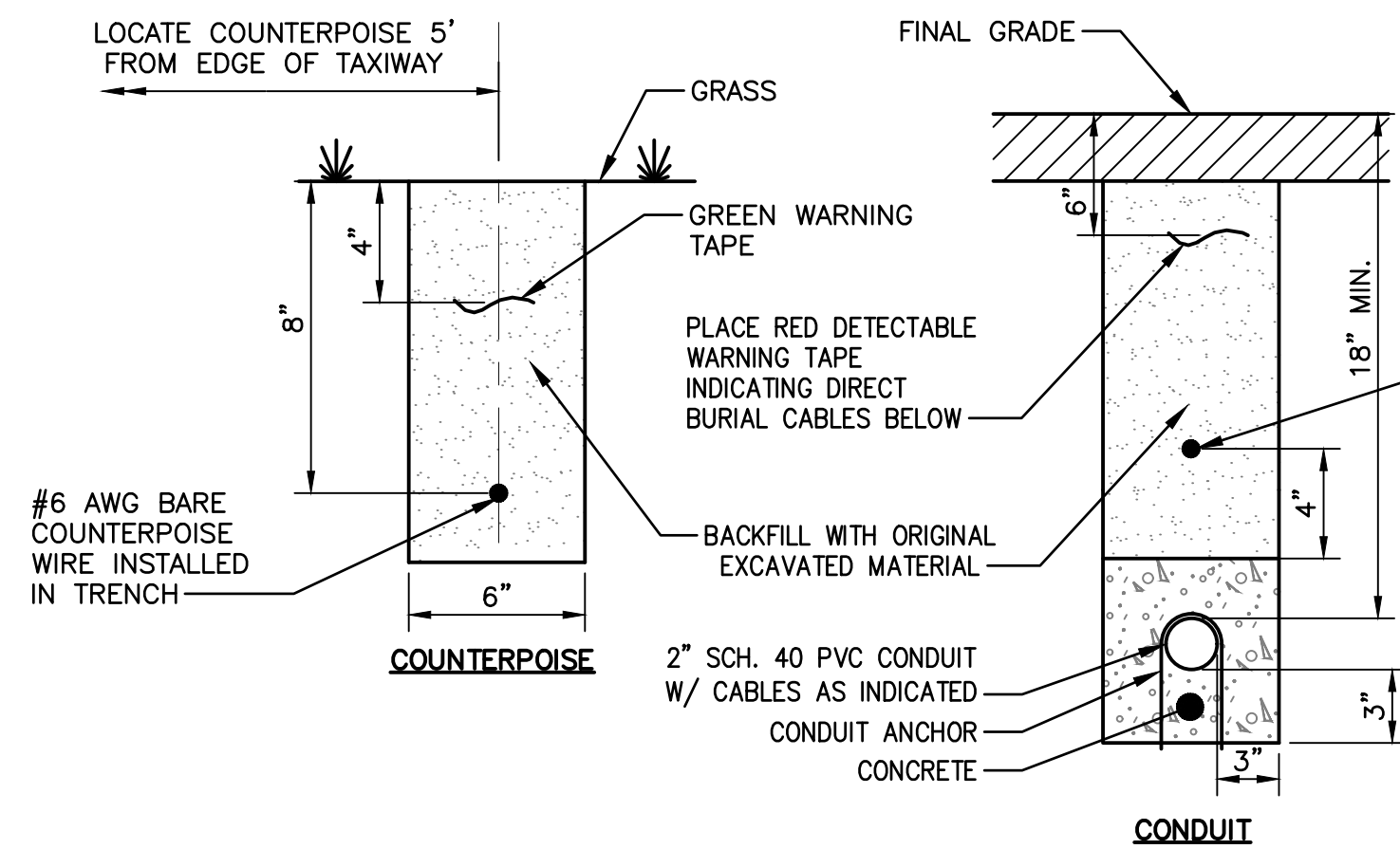
NOTE:
1. CONTRACTOR SHALL COORDINATE WITH MANUFACTURER OF CONNECTOR KIT TO ASSURE PROPER AND WATERTIGHT FIT.



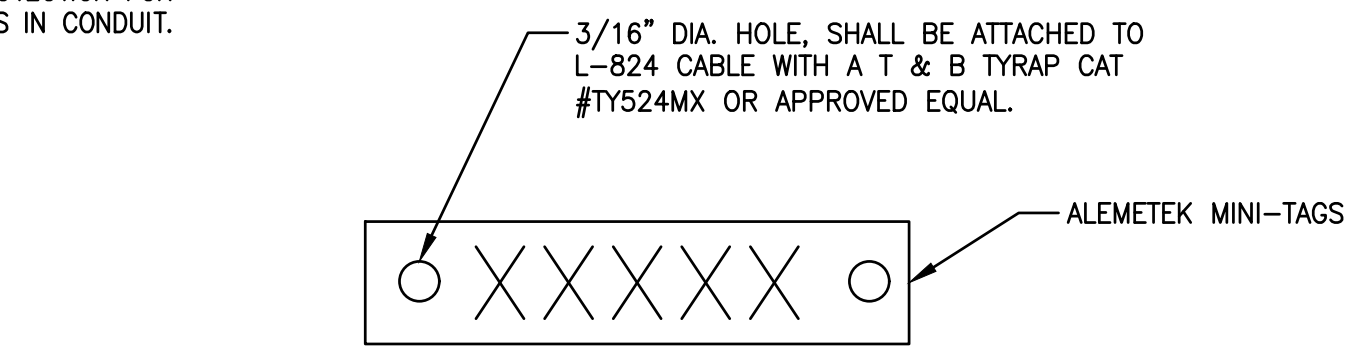
NOTE: NEW LIGHT TAGS TO BE INSTALLED WITH EACH NEW TAXIWAY LIGHT. COORDINATE WITH ENGINEER FOR IDENTIFICATION NUMBERS TO BE USED.



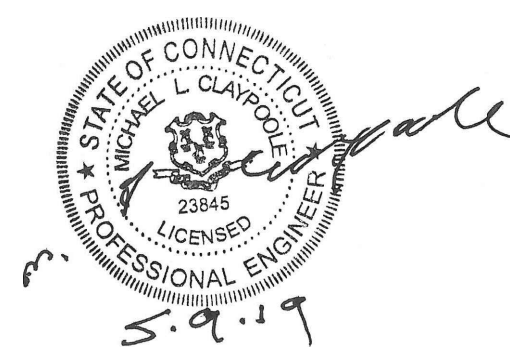
* - TAPE CONNECTOR WITH ELECTRICAL TAPE
NOTE:
1. HEAT SHRINK TUBING SHALL NOT BE USED ON L-823 CONNECTORS



NOTES:
1. SIDE OF TRENCH MAY BE SLOPED BACK TO MEET SAFETY REQUIREMENTS WHERE EXTRA WIDTH IS POSSIBLE



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MID-BASE APRON REHABILITATION

MERIDEN, CONNECTICUT

File Name: 195211190
Dwn: HAB MLC HAB
Dgn: 2019.05.09 YY.MM.DD

Title
ELECTRICAL DETAILS - 2

Project No. 195211190	Scale AS SHOWN
Drawing No. E.202	Sheet 23 of 23
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